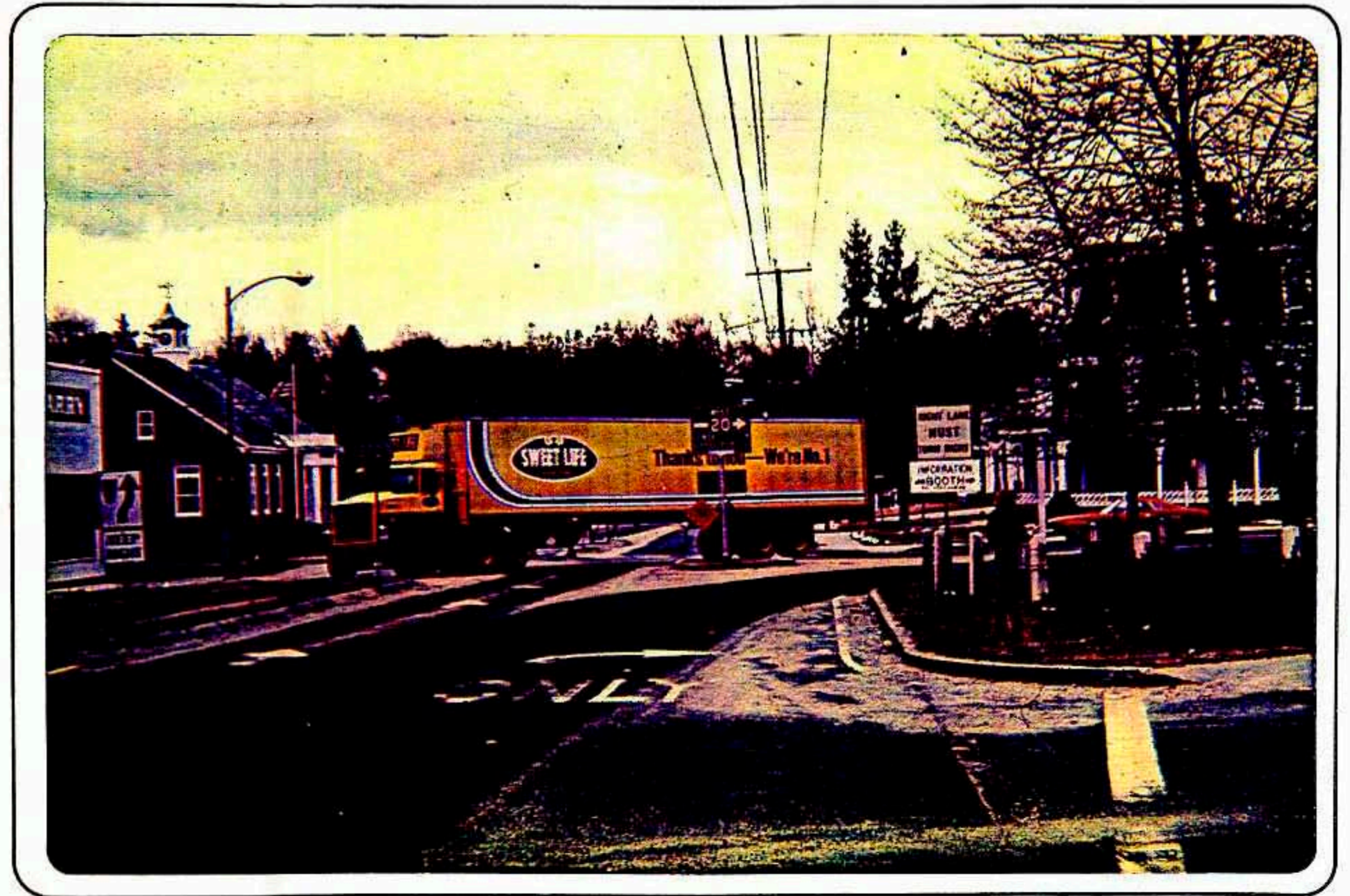


- 1: (W) View from (rte 20), 'approaching' our site of Downtown Lee: Park/ Main Street Intersection 'arrival', with a uniquely sudden view of Oval Park (Park Place) at right-- and then a strong right-turn gateway sense of 'entering' rte 20 Main Street.

In this very first image-- the truck (in wide swing turn) tips us an immediate clue of Lee's major problem: disruptive thru-traffic occurs at the expense of a safe, comfortable, attractive, inviting pedestrian-active environment with "increased economic/social/ recreational vitality". (All kinds of large to small scale, speeding or creeping trucks continue through downtown year-round, along with conflicting patterns of other *local, regional, interstate* general-- and the legendary *seasonal tourist vehicles* (which multiplies traffic). Much of the traffic-- especially the tourism flow-- passes through, disturbs, but does not patronize Lee-- but could if Lee were to enact such a plan. (Other traffic parks & quickly fills (and wastes) on-street areas, & creates congestion-- most disruptive during the beautiful summer & autumn ("Tanglewood" & "foliage") seasons.

* Depending on what the town wishes itself to be-- between the range of existing private *milltown*, or *rural shopping center*, or *full tourist town*-- there is challenge for all proposals to somehow "select and accommodate" desirable traffic, or "manage or divert" the remaining. (Proposals offer different goals for Lee to consider).

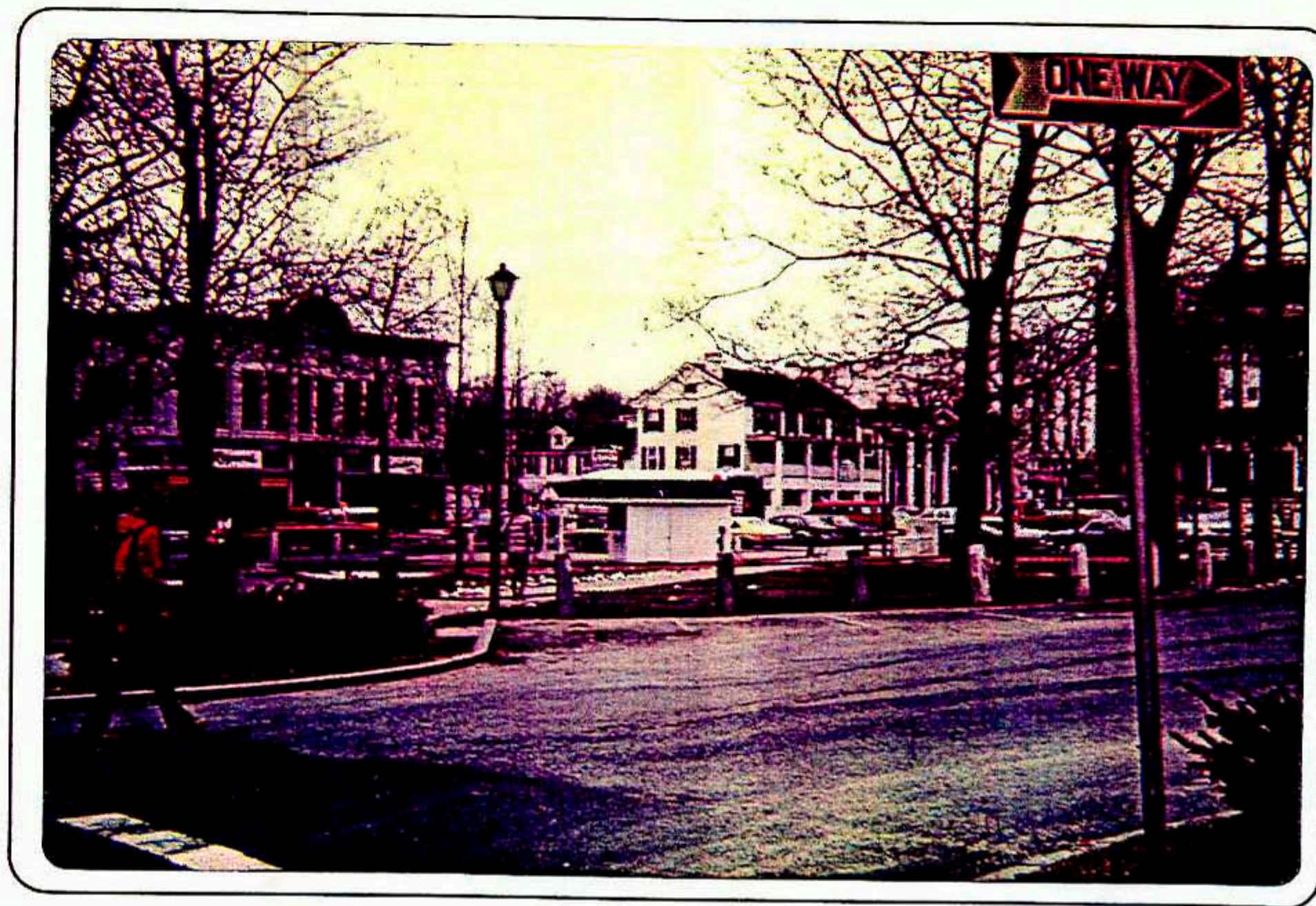
Note the regional sign to northward "Lenox & Pittsfield " ((from the e-w pike or south from (see wires) Stockbridge/Great Barrington/ Connecticut-- reveals Lee's prominent New England/ Berkshire crossroads locale; the lack of a welcoming local "Lee" sign though is telling of a comparatively muted identity for visitors-- especially those who pass through as part of the great tourism flow along the (e-w) Boston / Albany, or (n/s) Canada/upstate / Connecticut axi. At this south most end of downtown Lee, are made first and last impressions for travelers-- and repeated impressions for regulars-- where the sense of Lee as a welcoming valuable place, as further photos evidence: is much diminished into that of an overrun forbidding thruway, dominated by the "vehicular-distemic".



3: (NW) View from vehicle on Park Street ("Southend"),
thru Oval Park (parking area), with the tree-framed view:
across south Main Street, to the acclaimed pillared Morgan
House, and the busy Price Chopper backside entrance.

- Oval Park and its cherished historic remnants, are increasingly disrupted & fragmented by vehicles. A chaotic, tired, cluttered, unwelcoming impression greets as you arrive at a 'parking scene'— obscuring the fine architecture, human-scale quad, & potential activity. Accordingly, downtown Lee is sensed as a strained 'vehicular through-way', with 'urban erosion'— not as a special, amenable Berkshire place to live, patronize or visit. Here, and throughout downtown, are virtually no great pedestrian spaces or connections— and traffic/parking problems will only get worse. Lee's existing social, commercial vitality and physical strength *appears secondary to*, threatened or defeated by the unchecked demands of vehicularity throughout downtown. However— though Lee, a true working **milltown**, is essentially much different than upscale Stockbridge & Lenox— improvement potential for Lee's own interests— in a variety of optional directions— is very high.

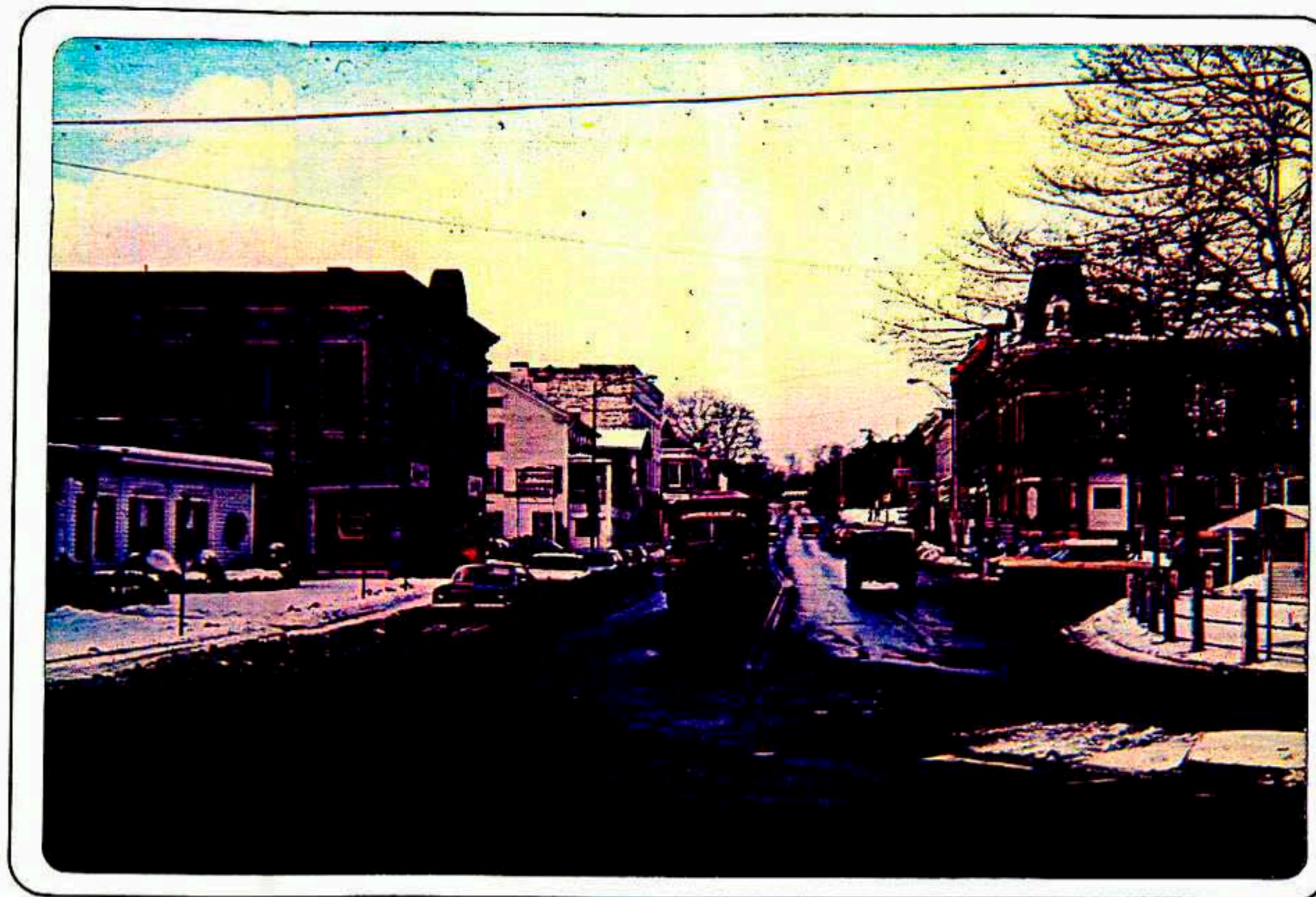
(At bottom right): wide one-way traffic entry to Congregational Church area parking. (At upper rt): Memorial Hall Courthouse. (At Midground): Oval Park's cherished, vehicular-threatened, isolated remnants include: important remaining magnificent mature random trees provide rare visuality enclosure & canopy; the granite & iron fence is historically authentic ("time-crooked")— demarcating a portion of the original oval carriage-way; part of the old green oval persists (though eroded & reduced by dominating parking); a compact monument fountain and elegant simple green benches (serve a few individuals, along walks) are well preserved. However space and year-round provisions for different individuals, group activity or community events (eg fairs, picnic, concerts, celebrations as would (eg) an open common & with bandstand)— are restricted. Note: Lighting includes only the underscaled type houselight, voids, & the nearby other-scaled street lights. There is opportunity for integrating several layers of distinct lighting types: entailing architecture, vs space /walks, trees, sky. (It, like all other features, can visually link & relate to off-site areas of downtown). The bus-stop shed is a good local landmark meeting-spot, & info kiosk for town & regional information, with storage & some climate protection. A large yearround pavilion of yesterday is also missed.



- 4: (N) View entering Main Street (at Park Street intersection):
of lower Southend Main Street: Lee's most commercial & enclosed 'place'-- is dominated by vehicular congestion. Pedestrian space needs to be *reclaimed*, along with the important vehicular improvements-- for a more balanced & 'co-functional' pedestrian/ vehicular, town-revitalizing use.

Foreground right: Oval Park, with good overhanging tree, bus stop, problematic-parking; brick Memorial Hall (caps commercial block).
Foreground left: Kelly Funeral Home (with its lawn extending the sense of the common), Park Building (Napa) with roof-crescent; Parallel parking; Price Chopper entry (past the rumbling truck).
Beyond is the enticing undulation of topography and punctuating distant voluminous bursting trees climax at mid & north Main Street.
((Main Street is downtown's primary open space-- a strong urban corridor of *three merging zones*-- (each demarcated by a plateau along the hill spine, each of different type enclosure, tree presence/absence, & degree of commercial (vs civic or res) uses: *Mid main* is the quietest most open zone; *northend* is moderate activity/ enclosure)). * Anywhere on Main Street, Lee is clearly evidenced as an active but fragile rural economic center at regional crossroads-- with the corridor enduring *several scales* of simultaneous vehicular use: ie an *interstate* thruway; a a Berkshire *regional* cross roads; and place of *local* town use. Main Street corridor itself has good proportions and articulations, of affinitive comprehensible human scale & a diverse good mix of cbd uses-- yet throughout disrupted & dominated by 'the vehicular'. Pedestrian provisions, amenities, trees are conspicuously lacking: downtown Lee is truly "pedestrian-place diminished" throughout.

- * This photo's *e-w cross* section reveals the generous width of street-space unnecessarily given over to vehicles that might be reclaimed. Intersecting e-w grid streets occur rhythmically, with access throughout downtown to unused parcels & open areas, for potential off-street parking-- to free up Main St pedestrian space. (A pre-studied *truck bypass* is an option to be re-explored as well). Note the good architectural unity of the large building blocks-- yet each completely varied; and, good proportions of mass, vs the space of corridor & some wonderful (but few) trees: are now intact! *Special note*: Overhead wires string across street-- interpretable as either negative, or positive (eg nostalgic, authentic, 'gateway' feeling-- allowing eg seasonal lights, ornament & event banners).

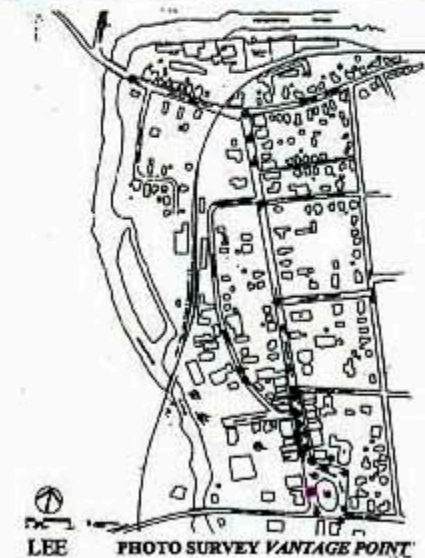


5: (NE) View from lower south Main Street, through the Oval Park "parking area" to two landmarks: the colorful brick Memorial Hall Courthouse, & Congregational Church enclave:

Lee's important, proud, active town buildings-- formidable yet human scaled-- with strong facade / walls, & diverse articulations, provide good balanced community uses, & make an impeccable positive-form architectural 'quad' frame for a downtown southend node. But all is dominated and obscured by the vehicular uses. There are virtually no trees or pedestrian provisions in this open sunpocket (protected in winter/ overheated & glaring in summer) even though occupying the very heart of downtown, beside the most prominent (architectural & community significant, larger scale) structures. * This location is highly visible, in the most central area in town-- proximate to everything-- with certain potential to connect new pedestrian activity and adjacent buildings and area/uses. Nowhere else in Lee is outdoor pedestrian activity so desired, so ready for a wonderful immediate transformation. Cost-effect of improvement here can be optimal. Here can so easily be redesigned (along with the larger green park area)-- removing parking to off street (eg Eaton/backside) areas-- so to enhance the 'whole' quad's role as a true, unified, functional and beautiful **Towncenter & Primary Gateway**-- that is pedestrian-active as well as vehicular-accommodating, for more desirable year-round value... connecting Lee's past and future centuries.

((Note: The parking lot has one-way existing curb-cut on Main Street (past hydrant), plus an entrance/exit on Park Street; Main Street (rt) has a narrow sidewalk, with bumpers overhanging. At lower rt: bus-stop shed, & random tree canopy in the eroded green area. Residential treeline is importantly evident (rt background). Other residential/ (unshown, to rt) defines east edge quad, near parking.

* Here, blue Berkshire sky with pleasant "slight wisps of cloud" gives the south node its dynamic ceiling-- with the seasonal weather and Berkshire landscapes promise richly varying spatial effect forever. On this early summer day, the contrast of bright open sun and dark acute shadows are important character givers visually and to experience; note the good aging density of (foreground) foliage-- elsewhere desired. (Proposals can enhance all this as well).

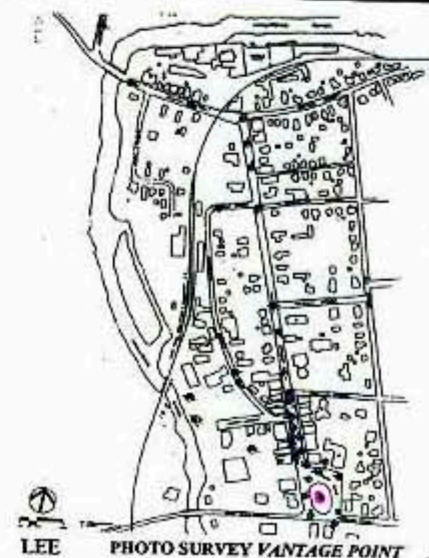
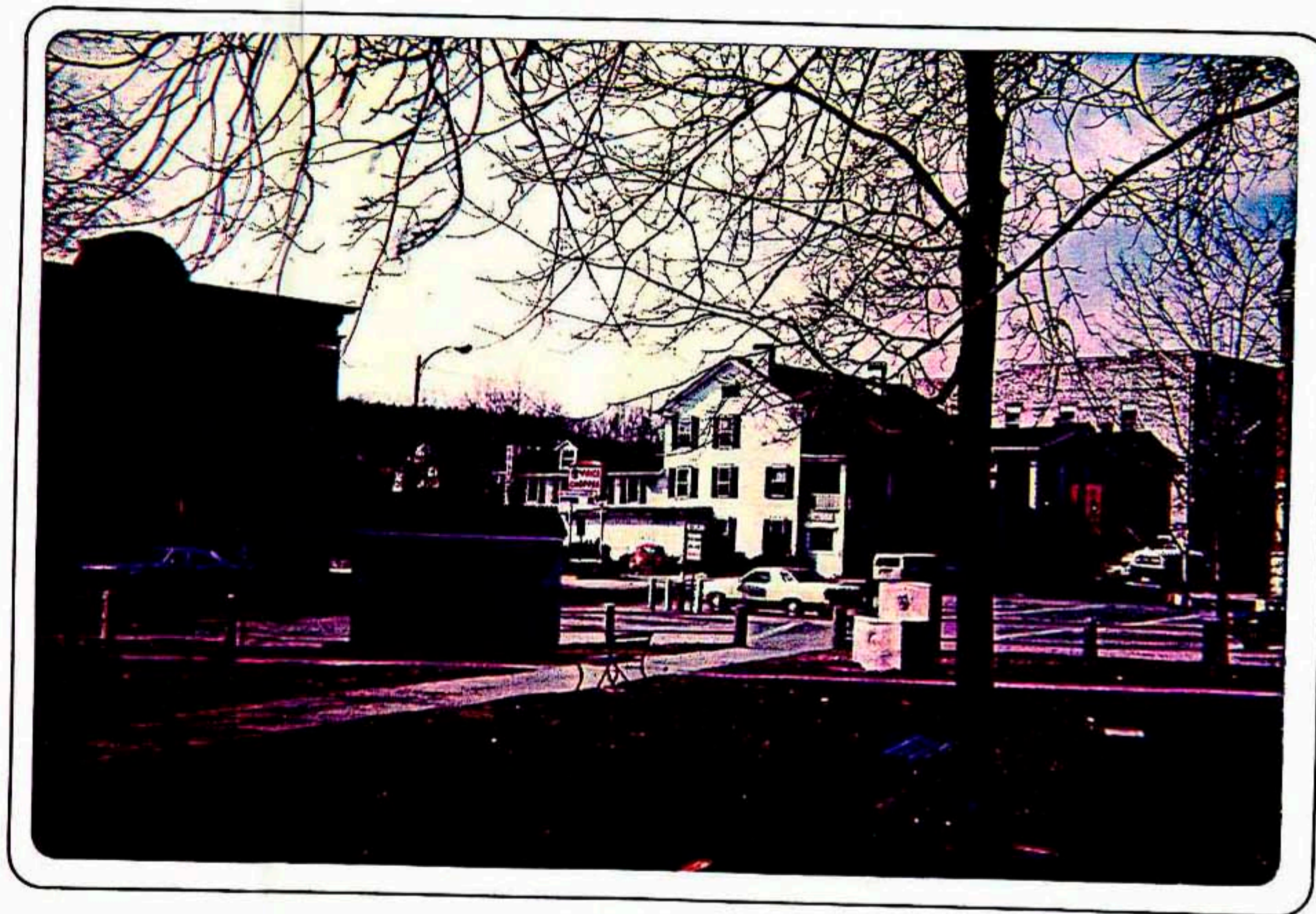


6: (NW) **Oval Park**— the remaining green area occupies the south corner of quad, with some very positive cherished *old park remnants* still intact— yet limited, disrupted, & threatened by vehicularity. The past calls out for a way to engage the future

(Foreground): The traditional lawn, cross-paths, a well preserved compact fountain monument, some remaining beautiful mature trees (with ready buds holding, and branch-textured windows upon Berkshire sky, often ice-glistened in winter, will be foliage-draped in summer, color tinted in fall); some elegant simple green steel/wood benches (serving local individuals, along walks), the original (mill-era vernacular, 'honest time-crooked') granite/iron fence, and landmark bus stop shelter (a meeting spot, with climatic protection and information board)— are all functionally, visually, and historically important. However, all is isolated & lost within the asphalt parking expanse; the area is eroded, reduced in size, limited in provisions: here its too restrictive for any significant range of year-round town use for more diversity/volume of individuals, group activity or community events (eg fairs, picnic, concerts, celebrations)— as if a larger open common, perhaps with bandstand. Lighting is also unresolved— (note the voids)— functional/safety/ aesthetic issue. The bus-stop shed is actually alienating on backside, & seems to block potential riverland view— but is a landmark 'object' as well.

* These cherished historic park remnants, and the park / southend as a whole will be increasingly threatened disrupted & fragmented by vehicles. A fuller park, within the quad, as part of the whole town open space system— connecting history with the demands of next century— is a necessary component for revitalization.

(Background): shows *south* Main Street: Price Chopper parking entrance, with simple colorful sign & nodding swan light; flanked (on left) by the "Park Building" (with roof crescent) and (rt) the renowned tall-pillared Morgan House. (Far rt): large (weathered) hardware block marks the important Eaton Street entrance. Note the appealing theme/variation composition: major vs minor 'block' proportions, of the different buildings; as well as the space between them, & diversity of light/shadow, openness/enclosure. (Both Chopper & Eaton St (e/w corridors) have potential for great pedestrian space/ trees and stretching sunlight— connecting Oval Park & the River, now absent. The swan light nods. From this spot in photo— we see the distant west of river forest banking— but there needs to be some creative way to really sense the Housatonic river system as part of the town open space system: true to Lee's historic natural/ manmade/ cultural Housatonic "river-narrative"— as emphasized throughout this study.



LEE PHOTO SURVEY VANTAGE POINT

7: (NW) View From south Main Street—
of Chopper entry, with Morgan House (newly painted),
& the unused prime sunpocket— (important for extending
New England seasonal comfort).

* Deep blue Berkshire sky— looks toward the Housatonic River-
land— and we may feel the great upper New England beyond.

This quickly filled parking area displaces any chance of pedestrian
activity. (There is opportunity to extend the Morgan House and
other buildings' spirit, character & uses to the outside; & to make
pedestrian linkage between Main Street/ Oval Park, the Eaton/
off-street parking area, and the river. Note: Of curious value is the
Morgan House's foundation planting— so rare 'on street', as to be
desirably iconic; and too the successional-mix residual trees
connecting partly river area. (Pruning up trees might allow a good
river-land view to/from Eaton-River-Backside Area & Oval Park).
* Hotter summer days to come will heat the asphalt and glare the
walls, wants connective pedestrian shading simultaneous with
vehicular function; A winter sunpocket can here be harnessed).
(At foreground right): the wooden (Morgan House) and the (left)
stone are a contrast of two proportioned "New England archtypes"—
that symmetrically frame the important lateral space to the busy
Price Chopper/ parking lot. (Traffic conflict piles up on busy days).

One Aesthetic 'interpretation': ((SPACE): The whole urban space is lateral corridor—
is simultaneously felt (as elsewhere in town) as both "added positive space" built by
the defining walls of objects, and as an urban clearing subtracted from forest.
The variations of OBJECT are in dialogue between each other, & with their different
relations to the voluminous background blue & cloud-streaked sky; at left a pilaster
stands and frames, with cutting cornice; (rt) Morgan gable & chimney 'peak'; trees
lace & bellow/season; streetlight reaches & cups space; wires 'silk'; planter sign 'stands'
((Proposals could appreciate the notion of strengthening either the added or subtracted
sense of this space, or the simultaneity. (See also larger Eaton area adjacent).

Special comments regarding lights & poles:

Simple standard swan street light— here cups the volume of blue
Berkshire sky— in gentle counterpoint to built rectilineals— while
rhyming with the biomorphics of natural surroundings. The swan
lamps are efficient milltown-vernacular honest— though iconic of a
more recent past than the older pole we see here or the aluminum
interstate scale one. Poles occur through all Main street.
The poles, though, vary: we observe *here* the older industrial
with good 'streetlighting' scale— era vernacular (historic
authenticity— unpretension— economic function— minimalism).
However, (and for better or worse is a good issue) the opposite
side of street includes large interstate aluminum poles instead;
and traditional, irregular wooden telephone poles and abundant
cross wiring strings throughout town); Also, a Local scale or
underlayer of lighting is also lacking in general:



((Some observers debate this irregularity as valuable to express the
meaningful layers (of scale) lights 'passing through' vs 'occupying
in' downtown). There is also the juxtaposition of *eras* at hand. In
any case, an added layer of 'local scale' lighting is still requested.
Note too the 'brush stroke' of wires— which has debatable very
positive or negative value as identity-character givers— as does
the out-of-scale anomalous sign-planter. (Such idiosyncrasies
can be interpreted as either 'personable & localizing', or instead as
'distracting, alienating, genericizing'. This is an important issue)).



- 8: (S) View from southside Memorial Hall Courthouse-- of Oval Park area, to the integral peripheral architecture, and then the south-distant Berkshire hills: these already nicely enclose the quad-- but are grossly obscured by vehicularity.
- * (The reader is invited at this very moment to imagine upon the photo, a wishful image of green & pedestrian paved space, filled with community activity, against the Berkshire horizon).

From here the old 'jewel' Oval Park is sadly revealed as lost in a sea of asphalt-- surrounded by vehicles on all sides-- sinfully fragmented, eroded in size, in threatened condition, and isolated from use. It is dominated by the sprawling clutter of inefficient parking-- a sure barrier to pedestrians. The combined asphalt of parking & roadway (which visually compound) is overbearing and prohibitive of more desirable meaningful outdoor use-- spanning the panorama horizontally, as well as to the distance across Park Street. In addition to poor location, appearance, and use-- the parking in itself is inefficient in layout (taking up excessive prime space, while actually limited in capacity), and fills up too quickly.

- * (In downtown there exist plenty of other potential-filled locations, behind church or at Eaton area can designed to accommodate off-street parking without violating a pedestrian opportunity here. Contextual pedestrian 'connectors', reaching outward beyond the street to all the other town areas is very needed). The very high flag (of many in downtown) signifies good community spirit (without actual celebration of park activity. The interesting angle of bus shed frees it as a special object /spatial articulator-- within the larger area-- but is arguably disruptive, and is limited to individual use. The community "bandstand days" are long gone in Lee-- they are physically defeated-- but the memory & social will to renew it is strong-- and will be stronger here & in the region next century.

- * Note: The Existing quad architectural enframement extends beyond to Main/ Center Street; (eg: Victoria; True Tire; Kelly Home porch is thematic with upper quad-area architecture). The important single landmark conifer, and mature random stands are pronounced (but limited) in park-- make a wonderful associative 'jumping-relation' to the surrounding Berkshires. ** The southward peripheral enclosing landscape silhouette is eternally dramatic (with a more temporal "connective spotting-frame of large conifers" & distant hill profile)-- though now obscured-- can easily be made visible to become part of Oval Park again-- to enhance the sense of very powerful whole space. How wonderful it would to reclaim this as a beautiful and useful Towncenter/ Primary Gateway. (See re off-street parking potential of the Eaton Area to relieve this area of excess vehicles).



- + ((Special Note: The landform base here is a wonderful "expansive" terrace floor within contactive ancient hills: a natural Berkshire node (transversed by an historically significant route)-- befitting and validating the existing man-made node-ing of important buildings. This natural base vs the manmade physical node-forming elements can however better combine for authentic climactic Gateway/ Towncenter use. *Proposals should respect the historic determining force of natural systems, for design clues per different town areas.



9: (W): View from the south wall of Congregational Church, to Memorial Hall Courthouse area, to across Main Street (with Morgan House at rt), to Price Chopper/ Backside area. This is a strong *lateral axis* spatial corridor— though totally disrupted visually/ accessibly by the vehicularity & chaos.

(Space is simultaneously interpretable as 'additive defined by architecture' as well as a 'subtracted forest Berkshire Clearing'. Proposals should strengthen whole sense of place in both regards, while accommodating pedestrians).

* Both Memorial Hall Courthouse & Morgan House (and their respective south facade sunpocket spaces) symmetrically well align where street crossing is needed— but are unconnected, and exclude pedestrian activity or view extension. Presently is visual/ functional chaos. Sprawling inefficient parking & pedestrian-disregard saturates both sides of street from oval park to the river. The vehicles give praise to downtown vitality (as we also respect the fragile economy). But there needs a way to maintain the bulk of cars importantly close to these vital destinations (eg Chopper/ southend) while creating desirable pedestrian/ openspace linkage.

* The distant river, with its westbank /landform trees (behind Price Chopper), nicely layers the west horizon— thematically enclosing the larger downtown with invaluable *Berkshire edge*— but also does not connect to downtown— and the good river area is hidden, ignored, and inaccessible. * There is important potential to relate Main Street & Oval Park's space, land, plants, lighting & pedestrian access through this axis to the Backside/ Chopper & River areas, (analogous to the potential Eaton Street)— while integrating new improved off-street bulk parking, and good pedestrian connections.

(Foreground photo): busy parking anarchically shares police, trucks, general parking uses, and a cut through walking pattern: needing resolution. HC provisions are also needed for *total* town circulation. (At far rt) fine entry detail stone & raised walk/curb. "Rid the cars".

(Background): Observe the lower elevation profile of Price Chopper building, from distance, expresses the e/w terrace drop to the river, while exaggerating a dramatic rise of west baking— as town edge. Main Street's architectural blocks provide the outer frame of view. The result, as viewed from Oval Park, is a good sense of rectilinear "quad space" (though now obscured by vehicularity, and unconnected for pedestrians, lacking tree or light connection)— "with the dynamic biomorphic river passing through n/s".

* A Special Issue all town areas: What is the "suitable form" of proposed structures?: In this photo, the dominant 'horizontal' (of Price Chopper & distant landscape) with subtle 'articulations' of verticals, befits a "river site, in the terraced, grid" (expressing horizontal-linear river & top/street forces). ((This good aspect is also found in the lines of other downtown architecture eg see photos the mill, or the longer commercial street-rows, & Arobi Building, etc)). Yet, on the other hand, 'vertical' buildings in relation to flat low river sites stand in good contrast, with exaggerated landmark-overlooking pointal quality; verticals also can desirably exaggerate a summit. So we must ask what form shall proposed structures take— regarding all function /aesthetic/ relational factors. Also, architectural materials can relate to 'nature' or 'manmade town', & (past or future).



General Comments:

- + It is important to keep the vista to the peripheral landscape open— (in both directions)— as a theme for all alleys and view corridors in existing Lee (although there might be option for some height expansion upon or extending Chopper, or flanking the building). (This principle of 'keeping contextual natural contact' with manmade environment— is relevant throughout all downtown).
- * This scene especially gathers a good eclecticism of various stone, brick & wood structures of several periods— depicting Lee's good plurality of very distinct 'evolved' buildings set within the original planned milltown "Main St at Riverside 'fixed' grid infrastructure"— with a residual backside paved area between in contemporary (eg vehicular) use: a true New England architectural setting, to better.



10a: (W): The Fine Morgan House , in Berkshire winter, with east porch, tries overlooking Oval Park— but is totally stifled in uses and appearance, and is land-locked by urban traffic.

The tavern-windows & chimneys hint the famed inn's cozy interior atmosphere (analogous to Red Lion, but more local)— yet Morgan House is without any setting for comfortable extension of activity, or to project its own value to contribute to "Lee as place". (Important: the building aligns with Memorial Hall Courthouse, as Main St. 'gate-echo' points; as well it makes a lateral gate to Price Chopper/ river area— though this sense and function is obscured by traffic, & lack of pedestrian space or trees. eg The front side-walk, where sitting is desired (especially in relation to Morgan use), is narrow, w/o crossing; southside is a prime sunpocket— that can become a pedestrian heart, complementary to and connective with the more civic Memorial Hall Courthouse/potential south side. Note: Morgan House has good rare intown (limited) foundation planting eg yew; adj clustered trees; sunside vines reach for sky, as do Berkshire hills— and are desirable in town— in complement to shade vines have been found in alleys and enclaves. * The issue of foundation/ domestic vs urban-motif plants or no plants is always important.

Other observations: Low fence is in rear; Poor structural linkage: on west & south sides (shed is questionable). The fine colonial architecture (with its own texturous successional trees evolved at building) stands beautifully against the winter glowing New England west sky— with the backdrop of (abandoned) river bank, and the west hill silhouette of deciduous & conifer— has sense of regional-historic authenticity— to better reveal.

The south wall/ sunpocket, when experienced up close or from within, is surprisingly generous in size (for its prominent location)— yet is given totally over to vehicles (that could use other Eaton lot area instead). There is great potential to extend Morgan House's, plus all adjacent building's program & other area pedestrian activity, into this space— for synergistic vitality.

((A relevant structural option occurs on the south & west wall of Morgan House to create for architectural change to integrate its interior uses directly with this space (existing rear door access)).

Note: The single-loaded parking design (as found elsewhere in town)— if not the whole location for parking— is prototype-wasteful.

* (In Far background): Residences west of river (upon upper terrace), as desirable, are barely visible: because much of westbank has good "landscape immersion": woodland (especially in summer) & topo buffering— is effective in many areas for proper privacy & 2-way view protection; but there needs further buffer treatment in areas of parking. (This residential neighborhood "protected but present" quality is important, as throughout town, to maintain a strong compatible mixed fabric. Also, direct walking access is needed for res to enjoy its proximity to the public— yet with the buffer-defensibility)).



(A special Wholistic Architectural Observation): In many good ways Morgan House is enhanceable as an *interlocutor* between an Oval Park area and potential river area: eg Morgan's *lineal deep porch* recession space at once views the park area, and echoes the feel of *lineal river corridor* "deep" channel space. Also the *undulating biomorphic facade* boards has a 'river-wave' kinship, as well as the spirit of a leisural park. In contrast to this biomorphic gesture, the building facade's hierarchy of grids (eg widows, columns; see with 11c) relate to Lee's counterpointing rectalinear formal architecture & street grid.



10 b: (W) View from south wall Memorial Hall Courthouse (unshown):

a closer lateral view to Price Chopper/ Backside area.

Lateral view-corridors, and longitudinal street corridors need to be open-enhanced and protected. ((See spatial analysis)).

(Here also Morgan House articulately frames views, and tries to focus a sense of lateral space— but without its own pedestrian space to engage activity).

As typical— the pedestrian struggles at risk, over snow, in want of a reasonable walkway. (See 10a comments that also apply).

(Note: Lee endures more intense winters (& often hotter, and shade-wanting still summers) than coastal Massachusetts.

Issues of safety plowing/storage & simmering summer walking area require a serious design solution. (See micro/climate section of the Analysis chapter in this study— info applies to analogous town areas))

Area-light atop pole provides important safety illumination; but need/also local scale lighting (working with primary pocket spaces and lineal connections as desirelines now suggest).

"Seasons greeting" sign signals the good spirit of Price Chopper for its many local/ regional patrons. Planter-sign survives careful snow plowing. The brick carries Chopper architecture— design (scale, context relation) are now debated as very or not valuable.

Single gray residence adds important sense of neighborhood— as do all residences on west side of river (immersed, buffered within the background landscape)— but it does not enjoy any landscape buffering from undesirable vehicular & store loading. Although, commercial/ mix of use for this or added structures may be an option here as well, as providing that any proposal respects the open-vista criteria expressed by citizens).

Residences occurring west of hill (with layered mix of conifer rows and deciduous trees, that vary by seasonal visual /spatial effects) look to Price Chopper loading & general combined Eaton parking activity— but have fair winter buffering in locations, and more so in summer. Note the relationship of hill to Price Chopper's roofline (and utilities), with some potential good views to/from Main Street.

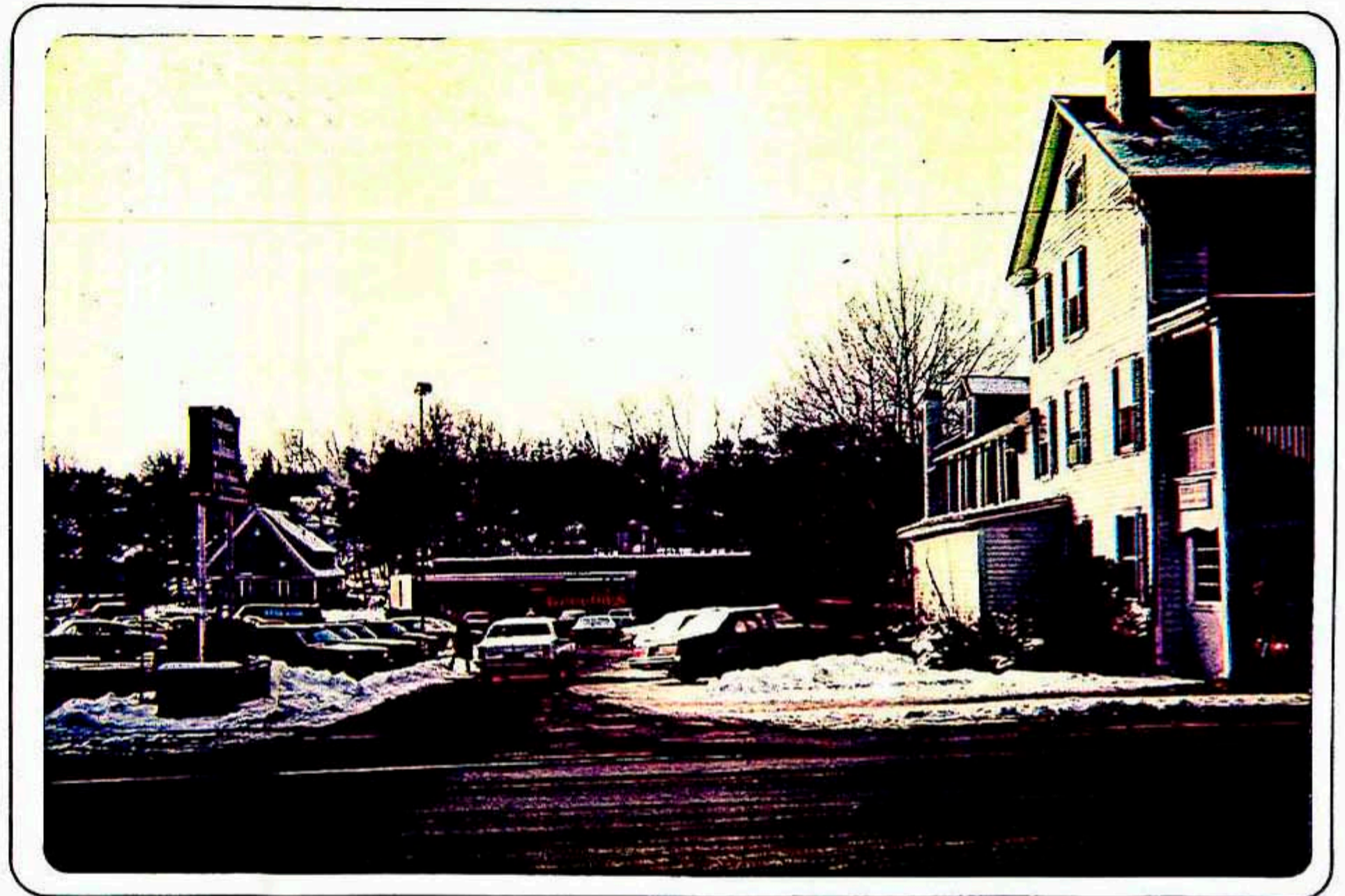
* **Special observational notes** (Compare space effect vs previous photo 10a):

It is experientially valuable that the very slight vantage change here (10b) effects such a significant sense of spatial change. Movement in town— relates to very significant small & large scale effects— that proposals should appreciate re space thematics.

* (See general note in photo 9, regarding the importance of

keeping the vista open to/from distant landscape, from many vantage points— for omnipresent senses of Berkshire contact. The town and designers have articulated this good point often)).

"Here established successional woodland defines the west edge of town along river, and makes a frame the backside quad, a whole— to make both an individualizing & unifying place— for the Morgan House, as well as for the other buildings, or spaces".



11a: (N) From Lower South Main Street (at east sidewalk):

positive & negative coexist: A good pedestrian/ auto balance is called for— necessary & possible. Also applies photo 11b

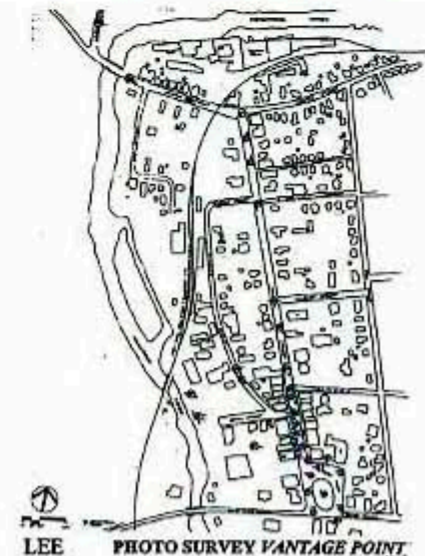
Auto domination in Lee is simultaneously 'local', 'regional', and 'interstate', with compounded seasonal tourism traffic (not shown here)— and always is versus **pedestrians**. There is usually a continuous stream of traffic through downtown. (See locus). **Angle parking** displaces potential extra space at southend; here, though a wider walk segment in Lee, still has a total sense of limitation & being channeled by parked vehicles. (Snow worsens). Crossing (n/s or e/w) is always difficult, unpleasant & often risky.

(Foreground right: Memorial Hall Courthouse & police. Fine detail granite steps & strong standing columns— symbolically/frames the cbd Main Street corridor with a civic presence— integrates the architecture, orients public 'up'— and functions daily, importantly. However, it squeezes the sidewalk, without use-space; HC & crossing is limited, with no building access here. There is curb cut to service alley (behind police sign) not used by pedestrians (with potential to connect adjacent areas). Sidewalk is also in disrepair; trees & pedestrian space/ structuring is lacking. Some smaller awnings exist, many underused; others lacking. (Compare this same spot as shown in this studies historic photos: many of the protective awning uses & widened walk vending space— old street 'market activity'— is now gone). Glaring summer exposure / winter wind tunnel in winter— with want of nearby sun & shade pockets. ((Morning/shade afternoon/sun is here, is opposite on other side of street— this n/s alignment has dynamic change through day, as well as the sunrise vs sunset shadow/ clearance. Problems summer glare & heat & open exposure. (+ But there is limited direct sun-blind for drivers on Main Street, vs the regions areas e/w streets)).

* Main Street, nonetheless, is an alluring positive community/ cbd place. This is Lee's busiest cbd area— with balanced variety of commercial/ municipal uses, articulate diversity of human-scale architecture with strong street proportions, vertical & /horizontal facade & street-line articulations. (Note the wonderful midground cornices, interlocking with sky and distant trees & exclamatory turret— embracing the street corridor-whole). The architecture and signage— with unified alignment & street scale— has richly varied vernacularity, at once emblematic of New England town, as well as distinctly local/ more private Lee. Evident too on mid Main are incredibly good large specimen great trees— delineating mid and north Main Street 'spatial/activity zones'— vs the openness of the foreground southend zone. Holiday decorations convey Lee's local social / business *spirit*. But, reclaiming Main Street space to make a more amenable, pedestrian-accommodating place— to broadly revitalize the downtown for the future— necessarily first requires a local and regionally relevant traffic solution. Ideas of a bypass, and new off-street parking (at Eaton/ Chopper backside, or behind the Congregational Church & elsewhere) hold promise



* **Special Observational Notes:** The midground "ancient" tree— with its prolific volume marking downtown's very center— makes exquisite dripping green & shade of summer, or a shining with sunlit winter snow, or a brightly exploding in fall! Other midstreet trees is seen fanning behind buildings, into a background forest. (Birds often accompany between these). (The turret, an old vital fixed exclamatory point, watches). Such dynamic idiosyncrasies preciously fill all downtown, within its strong simple infrastructure. However it is the dynamic activities of *pedestrians*— what is really repressed by vehicularity here— and what is most essential to wish for, most desirable for a balanced revitalized community cbd with increased commercial/ residential/ recreational/ social value



11b: (N) From Lower South Main Street (at east sidewalk):

Traffic includes "every kind of truck".

Pedestrians mingle in want of a place to walk, sit.

See previous page (photo 11a) for other comments. Note the (rare in downtown) crosswalk-- with inadequate HC connection-- has a limited connecting sidewalk area, low sense of low pedestrian priority. "Highway Paint" (vs other, or pavement) is questionable. ((Shows typical treeless area (see note regarding strategy for continuous vs contrasting (ie open, pruned, or heavy canopy) in a wholistic scheme. Sun/shade on e/w sidewalks are opposite see micro-climate problems)).

The sense of the corridor's strong *Southend* enclosure gives way to variously *canopied, fenestrated or very wide* open areas in mid/north Main Street. Observe (space, activities, nature, architecture) the articulation of strong basic simple infrastructure, as well as distant diversities. eg (rt) cornice lines interlock overall corridor space; the lineal (perspectivizing) of facades/ signs & street-lines, with vertical accents of overhang signs, nice peaking turrets, and distant episodic magnificent (but very few) trees, and far forest/hill horizon. The sense of important spatial activity zones dividing the length of Main street into southend, northend, and mid area)-- as well as intimate spatial articulations at each specific e.g. building spot on Main Street is there (but obstructed)-- to be enhanced with a wonderful *pedestrian/vehicular* design solution.

Efficient streetlight poles (see the older regional scale and the newer interstate scale poles) have alternating rhythm climbing street; But lighting lacks local scaling in places.

((Misc. Note: See notation elsewhere in text regarding the design issue of local Lee's simultaneous *interstate-scale* vocabulary of (efficient) lighting, poles & signage-- whether it is now alienating and generic, or is actually meaningful in expressing the dynamic 'regional motion' (of the river & pioneered roadway) and 'connection' among kin towns, and other remote Berkshire sources & destinations)).



11c: (W) View from key corner of Main / Franklin Street to :

A trio of special Lee buildings (lt-rt):

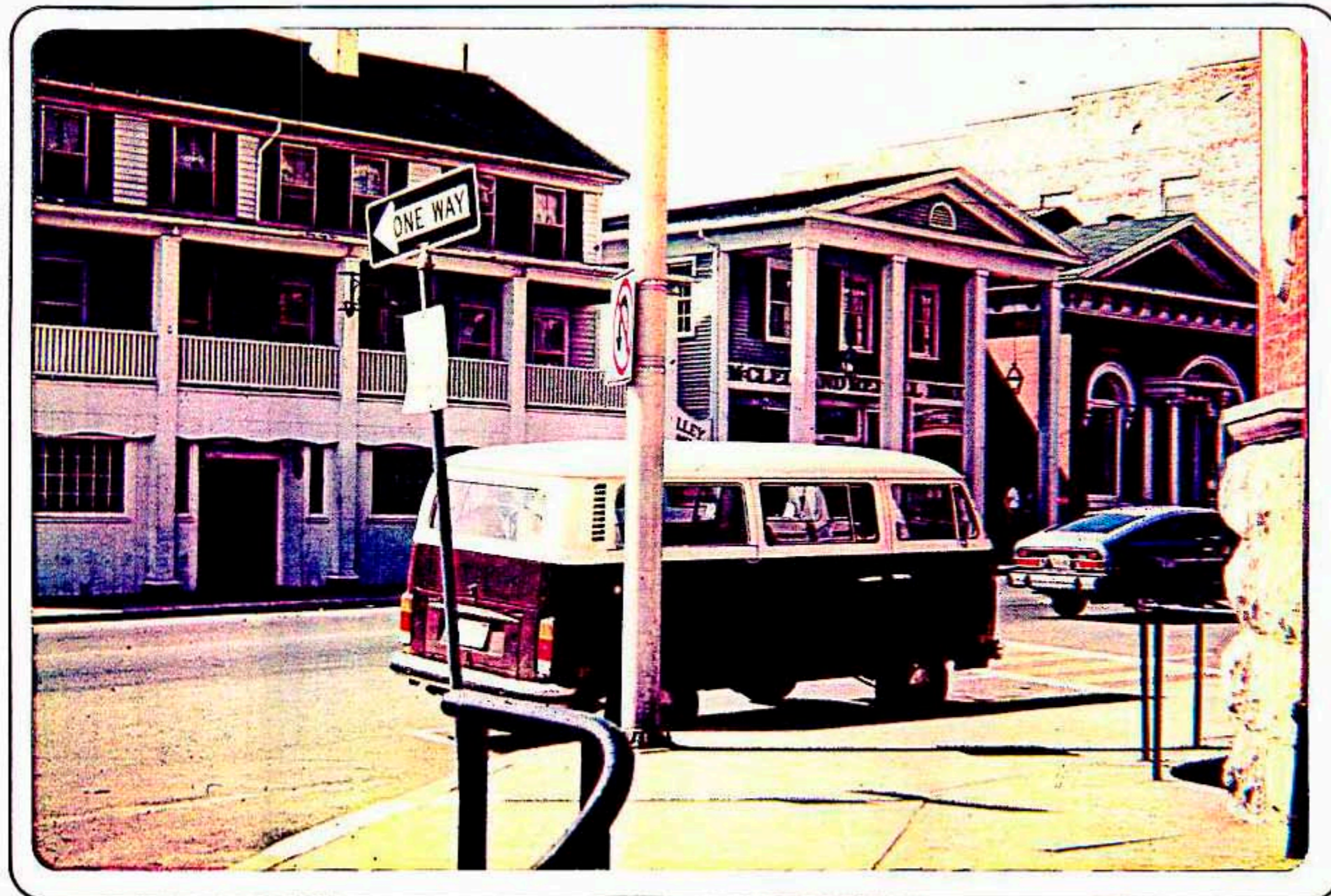
Morgan House (with porch facing park, that allows seasonal-extension comfort, which is lacking & needed in downtown);
McClelland Drug (with rare overhang protection); and
Lee Bank (with good recent brick facade; compare with enclosed historic architectural photo).

Fine distinct buildings-- and a strong collection, with relationships: Has special local historic use & architectural value Lee: These especially call for good pedestrian space to walk, rest, gather, vend-- extend their uses & presence out to town, and to, conversely, bring the town in. (Likewise for other special locales). The unified row has diversity of New England architecture styles & materials; and contrasting orientations (gable alignments; the step down roof line against northwest sky resembles hill terraces). Note the architectural kinship of columns & trim of Morgan and McClelland; and the kinship of form in McClelland and Lee Bank. (Good theme/variation 'interlock' exists-- physically and in mix of uses-- throughout downtown). Unfortunately, again, parking eats up space, obscures views and passage).

Architects Note: Observe the interrelating, *grid hierarchy* (columns, fenestration, ports) among these buildings-- vs *unique elements* eg undulating fascia board ((see note 10a about good "interlocutor effects" between Oval Park & town structure/grids, vs the river/wind-like wave)). Note also that Morgan House's porch picket fenestration and inverting window grid conveys a layer-sense of domestic prospect/refuge, plus has affinity to Oval Parks fence. * Hierarchy of grid create spatial/ territory-use layering. Trees --now missing-- could have a good analogous effect throughout downtown).

In foreground corner: is the limited pedestrian area of Memorial Hall Courthouse, needing connective crossing. The parked van at busy corner (dominates photo) is typical example of a single wrong parking location can singularly sever views, access, activity -- and destroy environmental space. (The one-way parking sign-- shows a key congestion point). Pedestrian provisions, trees, practical amenities again are lacking. * The opportunity for spatial/visual & access connection here with Oval Park is obvious-- extremely exciting to have good direct access to basement floor; see notable rustic granite base (signifies quarry era/ vs brick of Mill) and thematic (Mill-era alluding) 'local' railings; but 'non-local' eg aluminum poles (with 'efficient' lighting) occurs this comerside.

* (See note regarding the issue of *interpreting* these poles: as either being alienating and genericising-- or as being a meaningful authentic functioning expression of the *regional* dynamic-- (to simultaneously layer in with the local functions and signification).

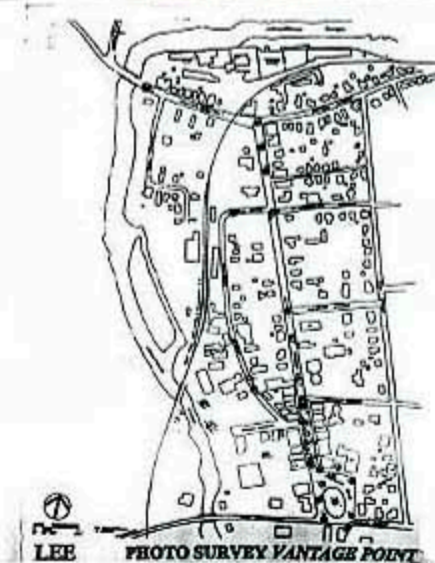
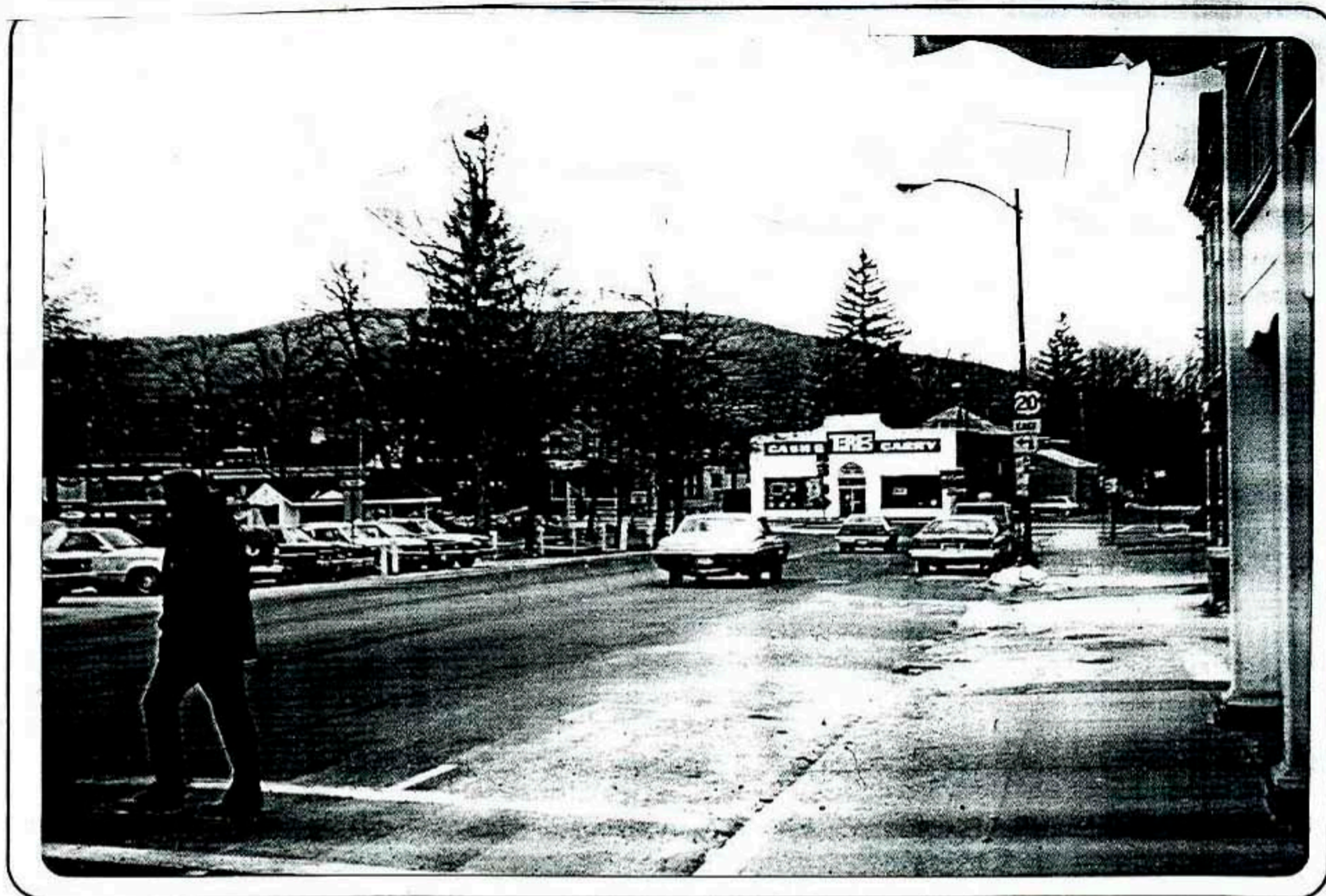


12a: (S) View of South Main St toward Main/ Park St. Intersection: good 'layers' of spatial enframement & enclosure continues south of Park St, created by buildings, plants, & landform—though obscured & fragmented by vehicular conditions—have a sense of linkage and spatial unity— to enhance:

TrueTire & Victoria make good *architectural* (commercial) south terminus of quad; (see photo 14a of east side residential arch). Beyond this are *magnificent conifer stands* (with jump-connection to Oval Park's conifer), *background forest* & Berkshire south hills. Oval Park (It) has sad barricade of parking & obscured bus-stop; sidewalks have inadequate width & condition, with minimal crosswalks. Parking/1hr occurs on right (e) side: is often is filled—(this hour is vacant, but still displaces potential walkway surface).

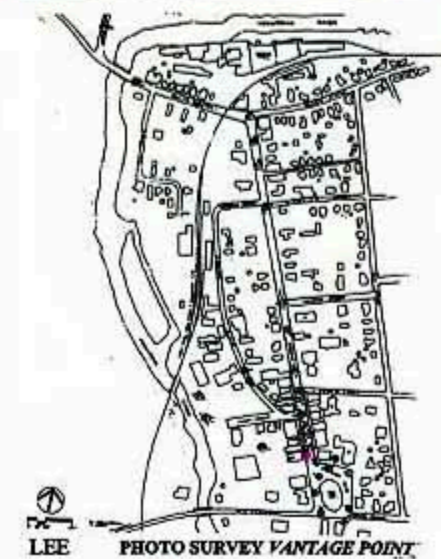
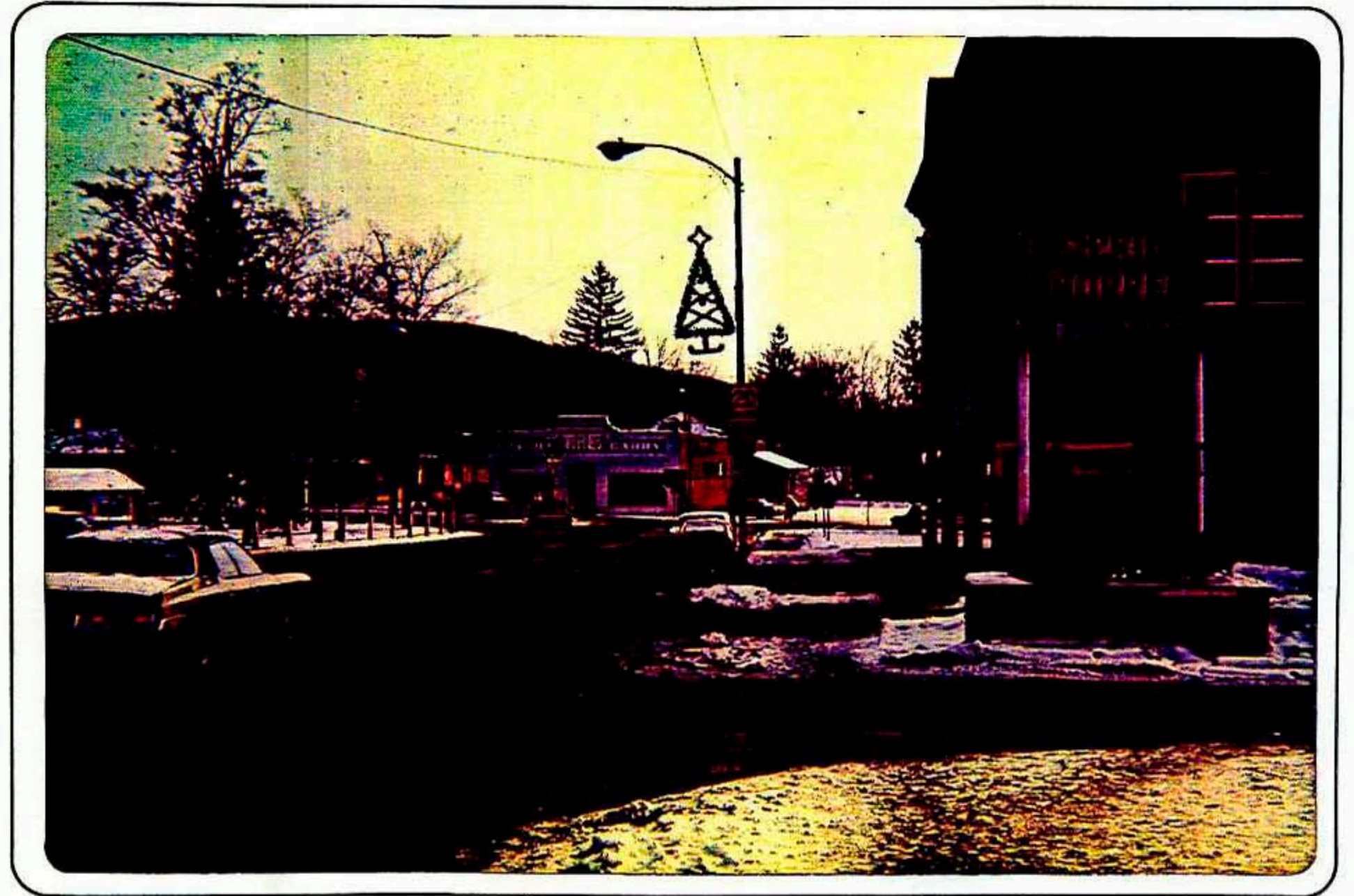
* Of added virtue here is that seasonal-change plays interesting spatial/ material effects on the sense of quad definition and larger downtown space/view experience of the enclosing Berkshire Hills. (Evergreen conifer & architecture make good dominant *fixed* space enclosure and 'datum' against the *dynamic* changing of deciduous trees, hour, climate, and human activity).

* Note the lively rolling profile of hill against the poetically sublime winter Berkshire sky. This geology is eternal while seasonal weather and vegetation, and activity play out the dynamic mass & space change that is integral to this berkshire quality of place. (A sunny summer day varies this to a voluminous deep blue atmosphere. Space recedes to distance at winter, with advancing of bright spring green foliage, darkening in summer bursting in color in autumn. The profile becomes far away, and disappears during sever fog or snowing-- and feels up-close on clearest still days).



12b: (S) Chopper entry way (with split 2-way, sign-planter)
 view to South Main— beyond is the powerful topography & articulate frame of large conifers, and Berkshire dusk sky on a quiet day: The predominance of roadway here is barrenizing when empty— and overbearing with traffic.

(At left:) Oval Park in snow— a cherished, imagable, historically significant "Rockwellian" corner of old Lee— is without any good pedestrian connection, and is isolated, visual-spatially obscured. Price Chopper entrance on busy days is a point of traffic congestion, with difficult pedestrian-crossing occurring in all directions of entire area; curb cuts are too wide, and displacing of pedestrian space— yet potential off-street parking access here, or with the Eaton Entrance, is a crucial consideration for proposals. Note also: Three-point overhead wire connection to interstate, juxtaposed with a postcard image of x-mass ornaments— depicts the paradoxical simultaneity of regional vs local scales. (See note regarding positive and negative aesthetic interpretations).



13a: (E) 'Everyday view' from South Main Street:

service alley between Memorial Hall Courthouse & east commercial block, to rear dumpster/ space & conifer screen.
 (Sunrise has valuable delayed effects through this alley).

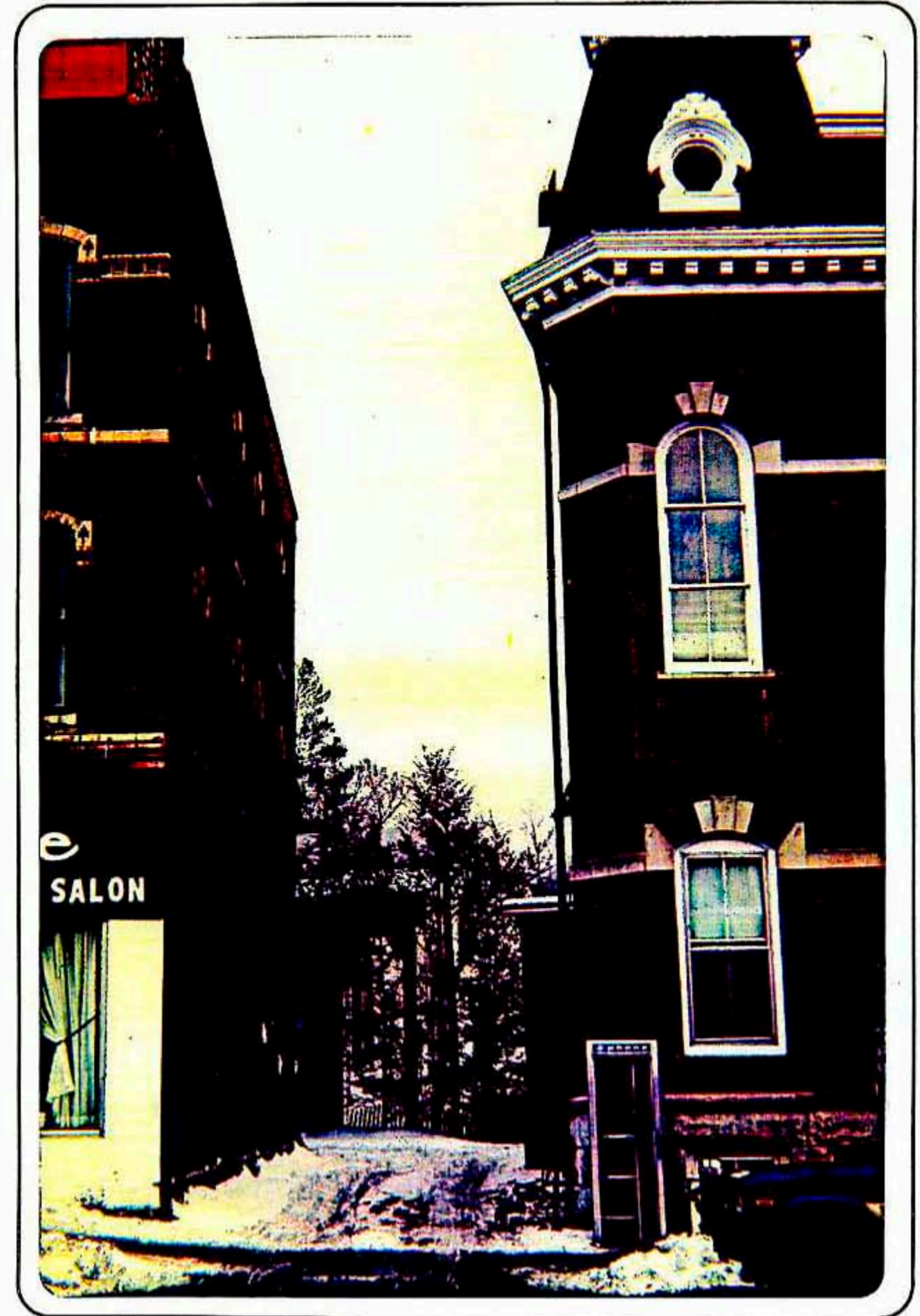
Important service needs and upstairs rear door access can be integrated for much better, inviting general pedestrian circulation. The Congregational Church— though dynamically framed on this alley axis— is hidden and barriered. Pedestrian connection beyond to Church, Oval Park & needed potential new off-street parking behind church is stifled. Main Street sidewalk space (compare historic market photo) also is limited, with 'squeezed in' (though conveniently located) phone booth (near bus, and police). Crosswalk is undesignated— but the pedestrian *desire-line* exists.

* Note (top right): Memorial Hall's symbolic "omniscient eye" window is a valuable example of the historic/ architectural richness that can be found along side the simplicity of many Lee structures. As well, the space of this alley, *and* the mass of architecture that frames it, each have *positive* forms— (each can give its single simple reading while the other is silent, or they can oscillate, or at once create a balanced "figure-background interlock")— all this brings richness to street facades as well as potential strong spatial experience & seductive connection between Main St & Oval Park. Functional safety lighting can also participate in the figure/ground 3d space (motion/ time) character— for more convenient pleasurable access. ((Compare with *photo 19's* smaller scale 'complex form' alley)).

* See note *Photo #19* regarding significance of alleys in Lee.
 Visual contact, and efficient, inviting spatial access is so important here in this most *enclosed* active part of town— to connect the river, Eaton area and all southend— to/from all eastward (the church, oval park, neighborhoods, & potential off-street parking behind the church: "diversity-in-unity". The contact of distant sky and natural woodland through alleys should be enhanced as a *theme, with variations*, throughout downtown.

Special Architectural Note:

The richness & permanence of deep red brick and local-granite articulation, befits the whole architectural quality & public prominent uses— in the best of *New England/ milltown* fitting materials, set in the *Berkshire* (brown earth & contrasting green) context.



13a

13b: (W) View of formal Morgan pedestrian Alley between Morgan House & McClelland Drug— a local downtown landmark-space— importantly connects south Main St to the Eaton/ Backside area. "Through the hearted eye of the alley arch, we find distant conifer on hill— and touch the Berkshire whole".

This alley directs people to the underused off-street parking, with a clear vista to Chopper facade and woodland, potentially orients— when used or just seen from vehicles on roadway— to the (now hidden & unused) river area. "Morgan Alley" is also a good *prototype* (public or private initiated) for connecting all Main Street activity— to the entire Backside activity off-street parking, River & other outer areas— and to view the *distant landscape and contact nature*. More such access is possible and needed throughout downtown to facilitate off-street parking to free up Main street— and for interconnecting all (commercial/ residential/ recreational and municipal activity) now fractured on east & west halves of downtown— for an organic revitalization. Note: Parallel parking blocks alley— compounded by snow. Crosswalk space here & alley lights are needed— a safety and space/aesthetic concern. (Safety lighting and obvious reading of ports, *beyond the alleys*— where they feed and empty— is also crucial). Snow maintenance in alley/ walks/ lots is a related design issue. Cornices, walls here give important (partial) weather protection— different microclimates for different locales.

Special Observations:

"The hint of sun light here pieces through the alley into Main Street corridor— and will exclaim the end of day into a darkened walkway. Alley light brings earlier sunrise, later sunset— extend the day— and knits in with the night". A town Alley-vista/access theme— of convenient protective space linking downtown areas & parking— and the near & far natural landscape (while each alley is a *variation*) can easily be enhanced throughout downtown.

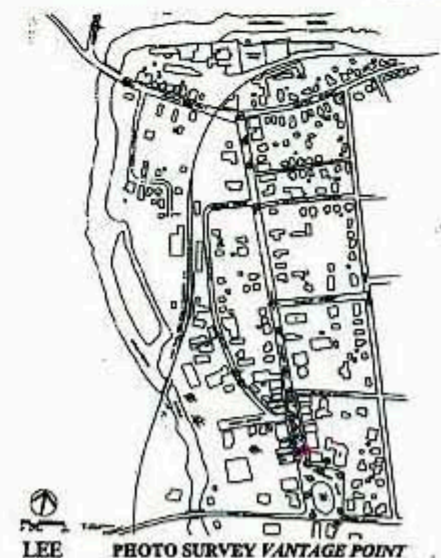
* ((Compare the interesting differences among all potential alleys in town. Eg This alley— with narrow & low proportions, is straight and simple with open lot view— is very different than the larger, more complex chasm-effecting (13a) Memorial Hall alley leading to the Church & most prominent community areas. Both alleys are more abstract and public than resident-mixed alley (19b). (See *general note in photo 19b regarding the potential significance of alleys in Lee*).

* The paper hearts in the McClelland window— as does the shape of the alley archway— in this town— indeed express the authentic warmth of an enduring local caring community.

Architectural Observation: The undulation of porch fascia & arch have a lyrical quality and orientations that could jump to river (wave) and oval park (leisure) associations (if a connected Oval park and Riverpark and connected Oval park were evident).



(A special Wholistic Architectural Observation): In many good ways Morgan House is enhanceable as an *interlocutor* between an Oval Park area and potential river area: eg Morgan's *lineal deep porch recession* space at once views the park area, and echoes the feel of *lineal river corridor* "deep" channel space. Also the *undulating biomorphic fascia boards* has a 'river-wave' kinship, as well as the spirit of a *leisural park*. In contrast to this biomorphic gesture, the building facade's hierarchy of grids (eg widows, columns; see with 11c) relate to Lee's counterpointing rectilinear formal architecture & street grid.



14a: (SE) View of Oval Park, in a "sea of vehicles", surrounding & barricading the old park's remaining mature tree stand.

Here is lacking trees, pedestrian space, amenities, local-scale lighting-- yet so desired. Instead, parking dominates the area: with visual clutter in all directions. The old park & whole quad area is truly pedestrian-forbidding for no good reason. Potential off-street parking (eg in Eaton/ Backside area, or created *behind church*, or other places) is a much better choice, for proposals to enact. (Great trees are evident from a distance as a threatened valuable remnant island-- read as objects in the quad, as well as partial space makers, a filter for residences).

Note: Residential /woodland yards define the outer east edge (at left), so have an extra concern in any redesign. (They daily face the presence of vehicular-related activity-- and want to enjoy a quieter pedestrian/ green Parkplace. (But add-option for select quiet commercial activity mix--is also a *long-term consideration*) In far background: The wonderful reposed hill, with its vibrant yet gentle undulation, pleasantly encloses on the downtown's south horizon-- to better reveal as a part of Parkplace's identity.

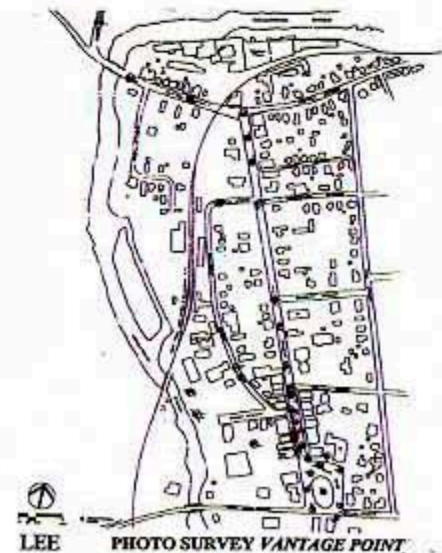
* ((Such potential *expansive views* (as help determine and define very different characters of numerous town 'areas'-- along with *focused views* through alleys at idiosyncratic points)-- can be enhanced as part of one's sequential-- and ultimately total -- experience of and identification with the town)).

Foreground: Long painted crosswalk with poor end connections; (other town areas have no crosswalks at all). Brick or stone crosswalk is an option mentioned by town to consider.

Note: Alum. interstate scale streetlight-- but no local scale lights. (See photo 5 & 9 notes regarding the issue of positive or negative interpretation of layered scale/ different era (eg) electric utilities).

Other observations:

- * *Memorial Hall's* granite foundation is quarry-vernacular, with brick. Its beveled corner helps 'turn' & integrate space of Main St with Oval Area.
- * Valuable winter *Old tree stands*-- especially in summer-- lend some visual filtering for residents-- although not enough to absorb the vehicular dominance.
- * From here, in background, you can *sense the meaningful rt 20 (to pike)road connections* -- entering or departing along the lines of the hills long repose: Lee is here sensed as a town at the crossroads of (interstate/regional/ local) activity. We thus feel a good charged (but untapped) viability of place.

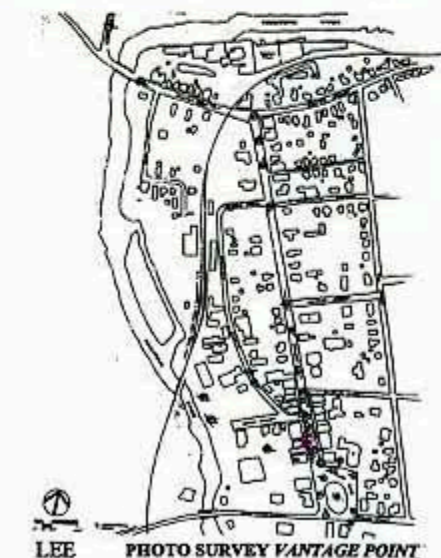


LEE PHOTO SURVEY VANTAGE POINT

14b: (N) South Main Street : Good busy activity and a diversity of personalited structures— but vehicles dominate (eg another truck) at the expense of pedestrian/ quality sense of place. Foreground, left: Fine Portico of Lee Bank, Jacks block (weathered side)— with widest walkway segment in town— but relatively limited in size & amenities, and in disrepair.

(Lee Bank columns reciprocate Memorial Halls granite steps/columns to frame the corridor with institutional permanence, amidst the retail and other town vitality)
 This architecture has good horizontal fenestration variation and some overhang, giving good spatial/ climatic recluse. (This side of street has more winter wind protection, and early sun/ later afternoon shade— desirable or not (depends on season/ weather)). Colorful signage adds vibrancy, welcomed here at gloomy hour. Trees again are lacking: (see note in photo 18 regarding tree/open interpretation); The interstate lighting pole (see note photo 5 & 9), the damaged trash can, & absent seating, adds to alienation and gloom here— it is a postal & bus stop, important to accommodate. Note: Irregular single curb here distinguishes a different identity versus the double-curb raised sidewalk area on east side Main Street)— though some curbs have misalignment/ failure. Convenient parallel parking (left side) has parking 'break' at bank, but min crosswalking provisions— variously lacking at dangerous Eaton & other street intersections for e-w or n/s crossing as well).

Midground photo: (left) turn at Eaton Street is opportunity for off-street parking— now underused; (At right) Rossi's vertical sign frames the division between Southend commercial zone, and mid street library/ residential zone. The library/ yard from here is evident, but its new wing is barren without trees, appearing as an isolated & uncelebrated place. (Library/area needs enhancement as a midstreet node— so to be 'hard & green connected' to the downtown's total pedestrian/ open space system). The addition's anomalous architecture needs landscape "absorption", and useful in/ outdoor pedestrian space (immersion) treatment as well. (Note the significance of great conifers beyond library/ mid Main Street area, and the deciduous / landform backdrop.



15: (N) View of South Main Street:

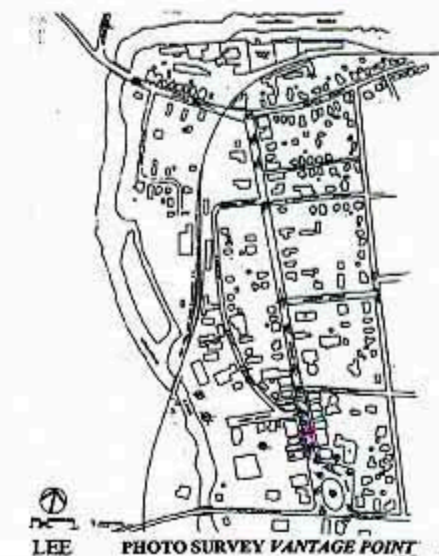
Angle parking (1-hr) results in narrow poor pedestrian space, limited use & comfort; difficult crossing. Bumpers overhang walkway. Treeless. "No place to rest, gather, vend".
 (Compare this very area to yesterdays marketspace, historic photo).

Another type truck (shows that the regional lumber industry is active, but) disrupts town. Walks seem "minimal functional"-- as if only for "typical urban street" quick-stop use-- not as a place to encourage comfortable prolific commercial or community/ visitor interaction. Note: the unique double-curb reveals that the area is a raised platform (adding a good unique pedestrian space attribute; but presents general & HC access problems Snow/ storage compounds spatial conflicts of this sidewalk/ parking/ roadway relationship.

Foreground Left: Eaton Street entrance left is congestion point;-- but can have good access to potential off-street parking. Street swanlight /poles often are only of the interstate scale, & wires occur overhead. (See photo 5 & 9 notes regarding the issue of positive or negative interpretation of layered scale/ different era (eg) electric utilities).

Note the significant qualitative effect of the good mural/sign at Lee hardware-- potentially celebrating a stronger Eaton lateral axis, in contrast to the perpendicular good Main street lineal facade/signs. Such existing color, composition, historic/semiotics & function all contribute to Lee's important "local-community scale" character (Painting/ renovation is due on some structures).

Background: Good large tree delineates 'mid' Main Street as the quieter zone. * Note for all photos: *In Lee there is the renowned omnipresence of powerful Berkshire sky, desirably juxtaposing the incredible composition of the built physical environment. ((This sky (here with hints of winter storm) --is always changing-- presenting polemic extremes; but it is always distinctly Berkshire in its strength of presence: different than elsewhere-- always clearer, fresher, truer-- than the city/metro areas where most people live. (The abundance and magnitude of Lee's evening stars will verify that).*



16: (NE) South Main East Commercial Block— important local uses:

Rossi's, Lee Pharmacy, Lee Hardware, 5-10; with upstairs office or resident— is the busiest, most continuous structure and spatially enclosing, colorful location of downtown.

The "raised platform" character of this (rt) sidewalk— (also on this side of Northend Main St)— not found on opposite side of street— lends to a special desirable pedestrian/ commercial place identity. (Several slight enclaves provide some inviting protective entryway). However all is too cramped in pedestrian space, and without amenities— for this most public busy area. Double curb/ steps/ and good vernacular rail occur in places, but need resolve elsewhere; HC problems also persist. This side of street has good afternoon sun; uncomfortable summer glare/heat. Better awning/use needed.

* Trees are absent (see last this page note below eg regarding signage).

Angle parking — which is limited in town to this area— (as opposed to parallel or selected-reduced) allows 1-hr quick pull in, but here quickly fills, and causes back up; as well its design displaces possible pedestrian space & crossing area. (Angle parking though has a compelling "hitching horse dayend visit" metaphoric quality). On-street service of buildings also adds to congestion here at times. Rear access varies, and its space is forbidding. New off-street parking provisions and policy-encouragement, with connective pedestrian access are needed.

* Architectural Note: windows, materials, cornices, signage— all vary, with unity of modules. Good rhythmic step-articulation of diverse structures & signage is valuable— with its variation against the thematic conifer revealed. Neither too regular, nor deliberately syncopated, the appealing rhythms of cornices and signage at once express the horizontal force of street life motion, and the occurrence of individual, permanent place. Also importantly, and as model for proposed buildings: two scales of cornice well occur: upper cornices encloses the larger street corridor; the lower cornice and signage, encloses zone of pedestrian space (though lacking actual groundplane surface & roadside definition). Below this is the most intimate (third) scale articulations of each entry space. Proposals may account for continuing or contrasting this existing (though disrupted or fractured elsewhere) scale-hierarchy.

((Special observational note: Various color & structural composition of 'lineal' signs, and within the color & structure of 'planar' facades— when visually combined are a good amiable stimulating complexity that is overall minimalist in logic, simple and appealing— befitting this local working milltown/center, as well as the idealized tourism image. Planning and evolved chance in this aspect (as in other aspects of Lee)— are happily evolved, historically authentic— and right for the future. (Proposals are challenged to respect all of Lee's old, while presenting the new)).

* Great conifers then articulate larger town space against unique background Berkshire sky— as do several other flags. Flag (far left) tells of Lee's historically enduring, strong community spirit.



* Sunshine glistens over the top cornices in morning, with strong shadow— then full sun occurs until dusk; other side of Main street is opposite. The e/w streets have their own cross-pattern micro-clim.).

* ((Regarding proposing new trees in downtown)):

Street Trees— absent here— would be fundamentally desirable to separate the parking, and define walkway space, vs street corridor. But such structures' (uses, facades, and the important signs) have a variety aesthetic/ spatial/ visual/ climatic concerns that call for a very carefully thought out planting (or non-planting) strategy.

General Note for all proposal areas: It is important to consider throughout downtown (ie see other photos) that the adding of trees versus allowing a contrasting (treeless) openness in areas, is a decision to be made in terms of a wholistic total downtown / contextual design. Also, the idea of lineal vs lateral or nodal definition— or, for example, new vs old-alley archetypes— are subtle planting issues analysis has found relevant).



18a: (S) View from Eaton/ Main, of South Main Street: Lee's busiest commercial zone during a lull between typical vehicular congestion, is here already at a functional/ comfort/ tolerance threshold for simultaneous activity. *Poetic conifers try prevail.* Architectural, historic-cultural, & ecological quality is here to protect.

(In foreground): Eaton intersection with typical conflict; and Price Chopper parking entrance (at distant midground right), makes this Lee's most disrupted area: of traffic vs traffic and vs pedestrians. Difficult and dangerous pedestrian crossing occurs n/s or e/w.

(At Right (west) side of street): here is the widest sidewalk segment in town, though still a modest width, and very short length. Size, amenities, lighting, better awnings are issues. The entire area is also treeless. (There is a design issue of proposing trees vs having contrasting open areas— in a wholistic downtown proposal).

* There are good articulations of protruding buildings on this west side of Main Street: Morgan House architecture creates good sub-pockets along the sidewalk— (but still limited width throughout for accommodating walking vs stationary activity of any volume). There exists a rich variety of established & recent, local/ regional merchants— strong architectural enclosure & diversity;

(On left, east side of street, at distance): *angle parking* unfortunately reduces the sidewalk width— more than in half compared to parallel parking, and 4 x compared to omitted parking/ selected crossings. Simultaneous driving, parking and pedestrian space is in need of a *balance* here and contiguously— as the new century approaches.

Special Observation Notes:

- * The larger, higher buildings' beautiful *architecture's mass* exclaims in the southend a desirable proportioned response to it being a lower elevation, larger spatial node. (Conversely in the northend).
- * The lyrical sprit of arched windows calls to/from the Berkshire Hills.

* ***The existing repose of hills and punctuated enframing conifer are wonderfully dynamic— yet diminished by traffic-domination.*** The Conifers and gables— a polemic of nature and manmade— have a striking form & distribution 'kinship'— vs the blocks & grid lines of the predominant architecture & landscape. Note the wonderful visual movement of these conifers— as can be found in vistas throughout downtown (yet diminished, lost by conflict).

((Here— variously at different times— might seem to anthropomorphically express eg: "standing" work, "respectful town guardship", "saluting, march", "intermingling play", or "celebrational dance": these expressions are analogous to a good community .



18b: (w) View from Main Street:

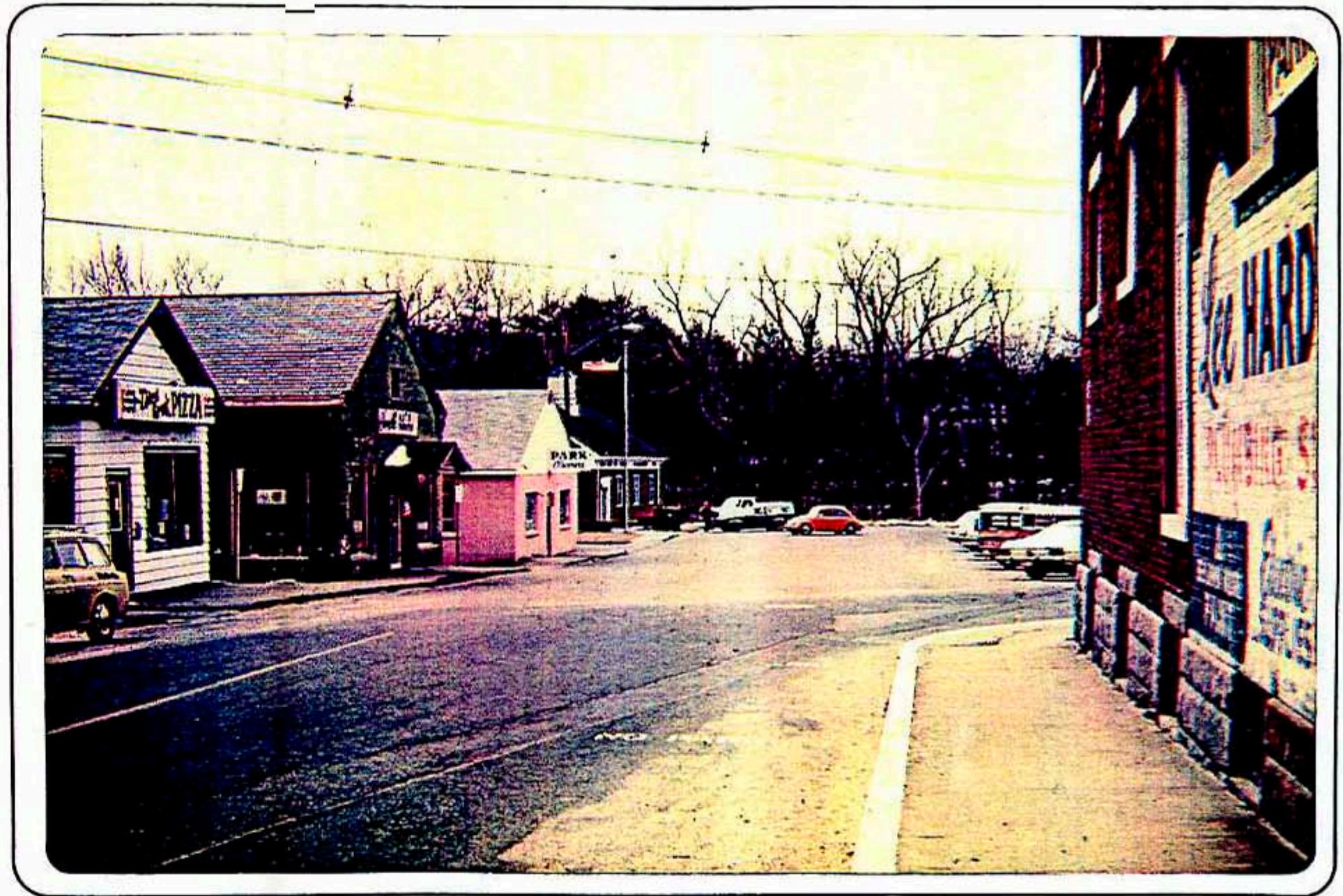
of Eaton Street & parking area, with paramount-located view terminating at the inaccessible, unseen, river point. Valuable urban space is unorganized, inefficiently allocated, unconnected, underused— but holds great opportunity for off-street parking & many expanded cbd uses— proximate to busy Main Street & the ignored neglected river).

These Eaton St. walks— in a major lateral axis from the southend commercial block— are narrow (for extra wide parallel parking)— with limited pedestrian space & inter-town connections between Main Street, the lot, and the river area. From here to the (yet unseen) river, treeless-ness allows oppressive climate extremes, and the area for pedestrians especially has undesirable senses of spatial disconnection, ambiguity, fragmentation— isolated from the 'significance' of the flanking Main Street & Oval Park— as well as river: it is alien, subordinate between those flanking areas. (Also, poor lighting feels and/or is unsafe after hours). Wires straddle overhead— fragment view and sense of space.

Eaton St does have its own good local scale/ use character inherently— and desirable idiosyncrasies of sun or shelter at cross with Main Street— and totally unique virtues of being the most powerful lateral axis to off-street parking— to the potentially the best terminal riverpoint in Lee (or 'anywhere'). Buildings and parking/ pedestrian uses on this street (axis) would reap value by proposed connections of the river and Main/Oval— by being between polar forces. * Lineal row local scale lighting, especially trees, useful walks/space would strengthen the sense good lateral axis connection to river, delienating of pedestrian and of vehicles (drive/drop-off/ or bulk park) in good co function.

* (Interview Note: In addition to the Anarchic wasteful parking pattern, and left over dirt expanse, it is simply unrecognized and underused by (eg) Main Street people. Yet the walking distance is proximate to everything). The overall parking area has large potential capacity (unshown: to right & left Chopper area with potential to integrate efficient vehicular, pedestrian or new building uses— and to free Main Street traffic and vitalize a new mixed use area in as connective riverside place. Vacant parcels are on either side of PO are especially available for added buildings/ or associated pedestrian space. Summer foliage condense the view distance and alters sense of space, as other seasonal changes occur interestingly. (Residents west of bank are more protected *then*— locales vary.

* *It is important to keep vista to river open— by foreground clearance and by new space, plants or pruning near river.*



* *Existing good smaller Eaton structures & signage* (in some disrepair) carry the good sense of downtown grid: Lee Pizza, Auto Supply, Park Cleaners, PO define the left side— with unresolved alley space between buildings, and Price Chopper/ parking area to left (unshown). Various good local evolved color: old pastel white, green, pink— and standard red— with theme of smaller gables/ local "necessity uses". This *loose-quad* small structures— in good complement/affinity with the commercial (larger scale) Main Street *corridor*— has room for compatible added buildings, *if* desired. (Service & dumpsters adjacent need resolve).

* Note: Lee hardware vernacular granite foundation— with windows/ basement use. (These also at once express: the vertical force of structure; the axis to the river; Berkshire/ Housatonic geologic dialogue; quarry 7built history; act of craftsmanship.



18c: (NW) View from Main Street:

Eaton Street : (a major lateral corridor from Main St. to, potentially, the river) has narrow walkways, & opposite microclimates: Rt side (Lee Hardware) sun; Lt side shade. Excessive street width and wide parallel parking on both sides, displaces potential pedestrian space; treeless;

+ "Berkshire sunset behind Housatonic woodland— at quiet end of day— is a unique part of living/working shopping/recreating/visiting (potentially more) in this town".

((Most of Lees spaces change their sense of characters— of size, enclosure, light, climate, meaning and uses— depending on the season/ day/ hour, their degree of vehicular or pedestrian activity. Proposals should appreciate & enhance the positive aspects of such dynamics, for each area of town)).

These flanking active-use buildings of similar proportions— brick in contrasting white vs red shade— make a powerful good fixed frame for (2-way) vistas. Dramatic changes in natural light— of the corridor, the lot area, and the wooded horizon well animate this corridors lateral/space: casting bold seasonal & daily type sun/shadows, between sunrise & sunset (viewed back or forelit). Climatic situation of the opposite walls can be quite extreme pos/neg, per season situation offering the contrast of refuge or discomfort on opposing sides. eg Right side has summer glare, but some winter protection. (Compare change in other photos).

((The Red brick (reads rich deep dark in shade) with white mural sign (rt), vs white structural (left)— well integrates functions & composition at a personable local scale. The good granite block foundation line, glowing as if by absorbed afternoon sun, is vernacular— (matches courthouse, mill areas etc.)— with relation to quarry history, and Berkshire geology. Here the built, permanent stone and metered cornice— point perpendicular to the natural transient moving biomorphic horizontal line of riverland)).

Powerlines straddle overhead the walkways— while shielding & slicing the backside area and potential riverland view (and hazardous in winter). (See note regarding how to interpret eg electric utilities (pos or neg.))? Dumpster/service in rear parking displaces space better used.

* A redesigned Eaton Street— with both efficient vehicular off-street parking access, and appealing pedestrian space/ connection from Main Street through to River, is possible). Functional/ visual expression (space, added trees, noteworthy lighting (row or wall)) here an urban corridor between the natural-river and the Oval area for simultaneous commercial, municipal, and recreational activity— reading



"Strong staggered planar quality of long winter shadow; at once mirrors the step profile of the (left) building mass, and enunciates the larger sites natural flat terrace ground expanse. (Shadows, as well as light— are a good part of the Berkshire milltown environmental character. Here the shadow step-profile variously condense and expand, as they change angles and size with shifting sun— at rhythms of hour, as well as season. This broad shadow, and lines of the (rt side) cornice and granite block foundation— all point perspectivizing angle to the river. The river woodland, in powerful contrast rises above the asphalt terrace expanse. The texturous forestland makes a vital line growing between the built environment and the backdrop of canvas sky. (This important existing spatial/visual character has inherent opportunities for proposals).



19b) W: View From south Main Street to Eaton /Backside:

Important neglected Alley space, to legitimize.

*"A certain slant of west sunlight moves across & glistens upon the shiney ice wet dark asphalt".
(Sunsets through this alley can range from a bursting flood of bright light, to fire red, to pastel glow—
with the darkened sidewalk walls in foreground). The vista here holds a single tall deciduous tree against
the background woodland and conifer row. * Through this alley between Main Street, and the far landscape—
the now hidden Riverland could be revealed, directly linked, & become a useful inviting part of downtown.*

Though now used informally, this alley lacks walkways, lights, and extensions beyond. It can be made very useful— to better connect adjacent buildings, new Eaton parking & service, and ultimately the riverland. (It already effectively aligns with a crosswalk— although without the desired sidewalk connecting expansion, or hc provisions (most problematic when iced.).

((Note: inside alley: Typical 'local' drainage situation (downspout, erosion/puddle): occurs throughout downtown can be a fine localizing characteristic, or problem)).

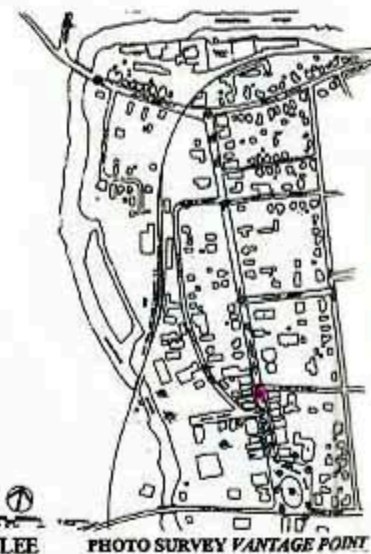
- * Of special note *here* is how *this* alley visually, functionally, and symbolically knits residential space amidst a mixed/commercial area (for a valuable diverse interwoven town fabric) that is so important to protect for a genuine uncontrived community/design. Here, the articulate personable interlocking of entry porches, (and the staggering of structures with lateral thrust indysoncratic spaces between)— perpendicular to the longitudinal direction of alley thrust, is desirable— and in good complement to other alleys. (Compare photo 13 a & b: Morgan Alley has a more commercial access & simple long space, open view; Memorial Hall has larger abstract scale, institutional public alley. See Theater, & Mill potential alleys).

- * Note how this alley (19b) space has a good *intimate- complex* type of 'field-background' interlocking form, befitting the areas more local, personal character. The space has an *anthropomorphic* feel, as well as *column-like* imagery— while cornices and walls create a pressing frame— with an inviting squeezing of spatial lateral force.

- * Observe in this photo, the good *backdrop woodland* (with the hidden unaccessed river area)— *is* uniquely well framed by alleyway and porch arcades: this can be better revealed, and needs pedestrian/ landscape extension to integrate contextually. ((The buildings and tenant-occupants in the entire backside desire their own pedestrian outdoor hard courtyard or green & seasonal garden yardspace// with revised (possible private) parking protection- in conjunction with alleys & other connective walks)).

*** General Design note for all "alleys" (refers to 19; 13 a/b, etc):**

As with other alleys and corridors (many places undiscovered) in downtown, such alley space opportunity exists to, in total, knit spatial areas of town— connecting off-street parking, as well as the buildings, service, and different pedestrian uses. Throughout Lee, alleys can be enhanced to thematically laterally- project from the built environment— to make visual or literal contact to/ro the natural Berkshire context. ("So always a glimpse of natural Berkshires appears from sidewalks, is what one can have; And conversely, potential glimpses and orientations toward urban Main Street from the peripheral landscape areas"). (Alley space experience in general is valuable: as climatizing, protective, intimate, directive with sense of vistas & reaching light, providing a transitional/ hierarchical relation to larger variously public town areas. Alleys are believed to be valuable as deep-archetypes in design types).



* 'Motions' of *e/w* alley, *n/s* sidewalk, and the *up/down* of particularizing steps: the dynamic space and functions here can be enhanced in its existing special forceful complexity— to express & serve an active interlacing of town life.

20a: (SE) View from Main/ Franklin Street intersection: the very substantial East Side of South Main Street Commercial row.

(Behind these buildings we see the Congregational Church 'steeple', with its contrasting white slender form, piercing distant winter sky-- alone asserting the unseen presence of Oval Park. (The steeple is an omnipresent datum).

* The reader should track this steeple in the remaining photos for a orientation to downtown's intriguing dynamism of place.

Here is downtown's largest scale most continuous, enclosing, good commercial row : a most significant extroverted "hard-formal architecture" of established uses (in complement to the good, more intimate wooden west side of street): a 'lineal anchor' for the town.

(Foreground left): is the good familiar "Rossi Restaurant/ Lee News block" (with Lee Electronics, Bee's Clothing, & upstairs offices).

(Adjacent structures): Lee Pharmacy, Marble Hdw, 5&10; boutique; and continued upstairs office space. (Some rear access/service).

((Note: Open zoning wisely allows commercial or residential upstairs as free-market forces desire on Main St type cbd areas)).

(Far right):The *Municipal Memorial Hall* Courthouse terminates the commercial row at Oval Park. (Conifer, distant hill in background).

* Architectural note: Within in this *unity* of the row, scale, block-proportions, and publicness of buildings-- are the very appealing shade variations of brick, and articulations of the entry enclaves, & rhythms of windows, cornices and signage. (This brick & stone is vernacular with the Lee's Mill/ main Street new England history-- stone in all downtown is historically rooted to Lee's own quarry industry as well. "Very important diverse mixes thrive, in very good architecture-- calling for quality landscape architectural space to balance pedestrian desires and vehicular needs":

Pedestrian space is limited, restrictive for such an important area. Winter snow creates additional barricades. Painted crosswalk is without expanded walk or HC; (Though crossing prohibits parking, it is asphalt not designated for pedestrian space use either). It is a treeless area (see note regarding tree/ open/ pruning strategy, & sign visibility proposal-issues). Important but limited awning use.

(Microclimate Note: This side of street has morning shade, afternoon sun, to warm colder days; but is hot glaring summer sun; is more winter exposed than west side of street, but has a few idiosyncs of wind pattern, & enclaves for some winter protection). Interstate street pole (midphoto) has effective lighting; local scale lamping is lacking; (See prev note regarding scale/era 'layering').

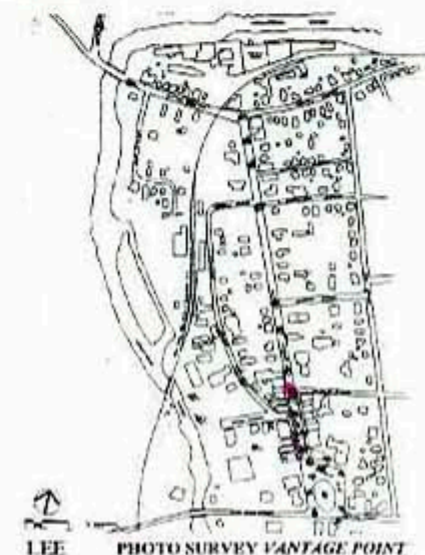
* Note the colorful signage here a balanced 'complexity' of three uniform types: facade signs; extended (overhang) horizontal signs (news room & Pharmacy); and extended vertical sign (Rossi). Yet it overall is pleasing simple: for legible function, and aesthetic stimulation (without pretension). The overhanging signs create good subspace markers along walkway. Rossi vertical sign also depicts a special historic "Old Main Street" era-design significance (see night lit), and makes a defining edge frame between the whole southend Main Street versus the quieter mid Main library area (unshown to left). These serve the town very well-- signifying the commercial vitality and in a functional/ aesthetic collage.



General Note: Regarding all existing signage downtown

The above good variation "types" of local-scale signage type-theme/ vocabulary occurs though incompletely, throughout downtown-- along with the interstate traffic signs ((Eg on west side of street, slightly smaller range of hanging or facade & with window office signs, the free standing chopper sign and larger Eaton street wall mural. A few mid street post signs, the northern and Center Street)).

In sum, most of Lee's existing local signage (though some need maintenance) is deemed quite *positive* for milltown/cbd use and image-- though improvable in some areas (especially if the tourism strategy is engaged); However the interstate signage, and some utilities (see note regarding interpretation & 'layering')-- and lack of off-street parking/ policy signs are deemed *questionable*.



20b) (E) View from Main/ Franklin Street intersection:

Franklin Street (one-way east), residential/ mixed character uses coexist well— a 'merging interface' to be protected, with opportunity for interlocking-use types of pedestrian space.

(Foreground Rt) :Main Street sidewalk with hydrant & painted crosswalk: important location has undersized, diminished pedestrian treatment— that continues along the street corner/side.

Portions of right side sidewalk are ill defined or absent;
(Auto is parked upon walk area. Snow & service add conflict).
 No ped amenities or trees. (Far rt) is the cherished *Rossi Building*.
 ((Client and patrons desire more outdoor space, & facilities).
 (Note: The exterior stairs to upper floor Rossi are a good prototype for proposed new or existing structures elsewhere.

Adjacent (steeple) is *Saint George Church*; with rt turn service access between them, (at pedestrian) can connect to vacant parcel (with *potential for "north of Congregational Church parking"* deemed available by client).

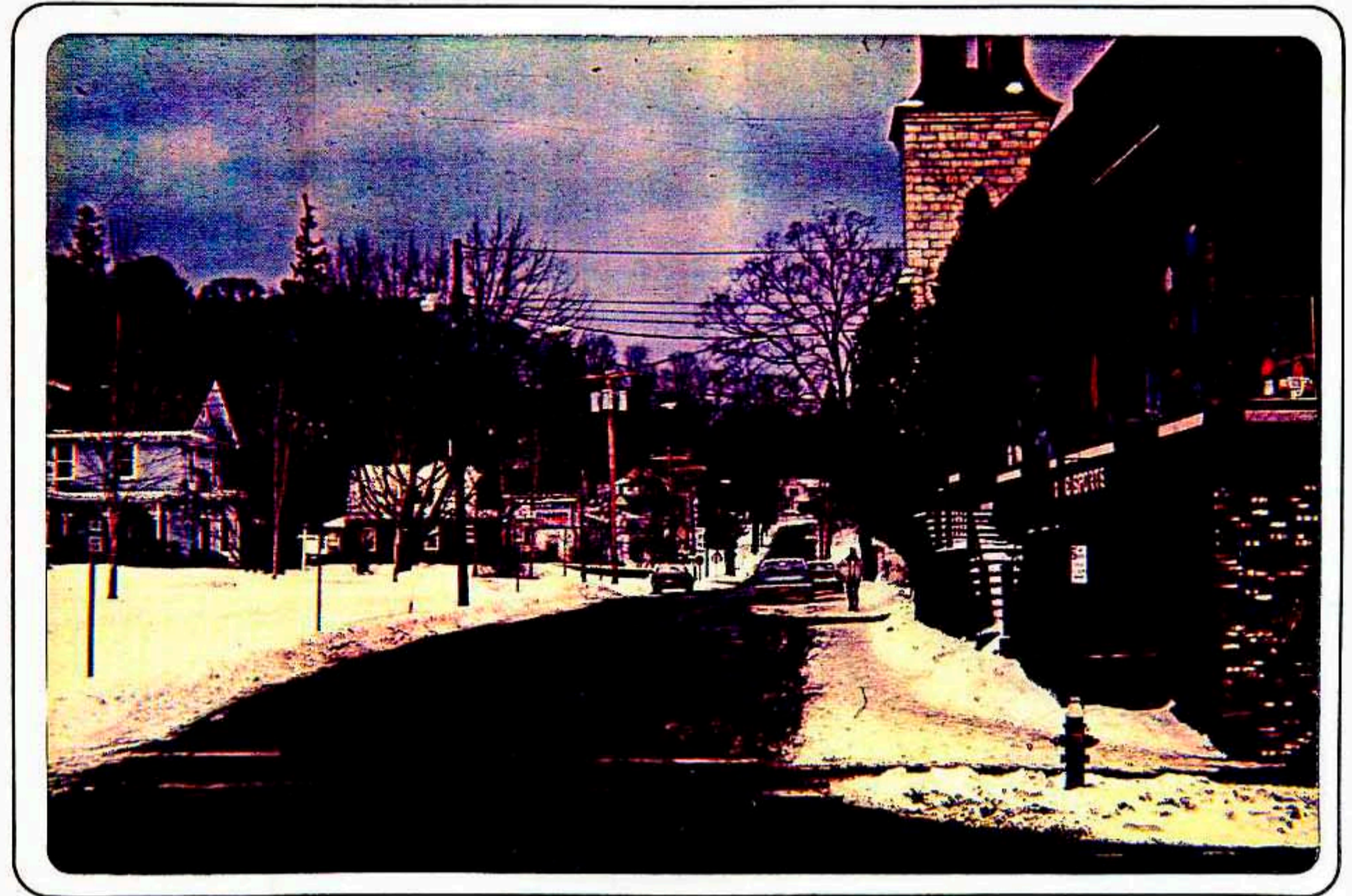
(Foreground Lt): Open *library-yard*, then *appliances*, with treeless sidewalk area. A multitude of wires connect directly overhead.
 (See note regarding the positive or negative interpretation of old utilities).

(Remaining street): is *residential*. Two sidewalks are intact; plus convenient parallel parking is throughout— assists commercial/ service/ or church spill over on busier days— but needs sign regulation to control future traffic excess. (Good strong large, but few, deciduous trees, and spotted conifers are midstreet, among smaller curbside trees, absorb poles adequately. High Street—(a poor candidate bypass option) is seen midground at stop sign— is an important local neighborhood n/s street area to protect— with resident yards, school /grounds; east forest goes to background.

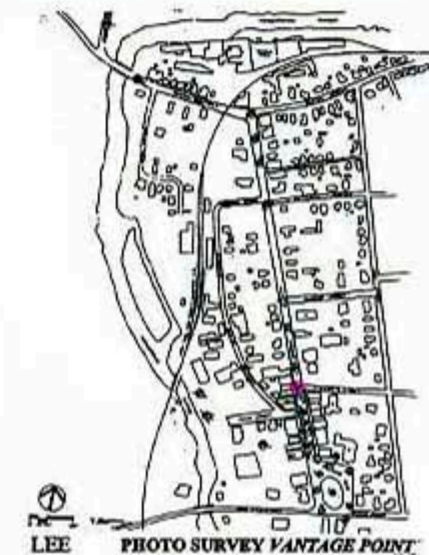
Here: the wooded context— layered immersion of dark texterous branches, among conifer accents, fan across deep blue sky above snow covered yards— form a comfortable winter scenario for a neighborhood— with merging many mixed uses— at the edge of most commercial Main St.. (Compare Academy St's more residential).

• **Special Observational Note:**

The gentle long topo e/w rise depicts Lee's Housatonic narrative of ancient river terrace— in dynamic with the dominant north/south topography of Main Street— continuous with the powerful contextual Berkshire Hills. Again— (as other early historic photo showed)— landform is everywhere, making distinctions as well as interconnection at all scales— essential to the sense of place: ie the sense of entire region, of the unique downtown whole, and every sub-area and street. Proposals must respect, and take a major authentic cue from that.



- * The diverse mix of materials (stone steeple, with corbeled arch, brick, and wood) represent the streets diverse mix of quiet-compatible uses (institutional church, commercial, and residential mix). The interface works well: protect it. Other side streets vary.



21a (S) View from the beginning of mid/ Main Street, note the lower expansive south end's topographic "*natural terrace dip*", coextensive with the more active & structured-enclosed built south node: natural landform therefore co-delineates and enforces the sense of different spatial character/activity zones in downtown here, (as it does uniquely in other town areas).
 ("As we leave south Main St, we sense the less commercial, more open and quieter civic/residential mix area of commercial Main St"—although is much overwhelmed *with traffic* on busy days/ tourism season).

Here... a peaceful moment of week/year, in contrast to traffic time, allows brief emergence of Lees "inherent" strong built/natural spatial infrastructural & contextual quality. (See Spatial Analysis diagrams). This photo's cross-sectional aspect shows pedestrian walks & space are overwhelmed by roadway. Trees are lacking in downtown, vs the prolific Berkshire natural context. (See note: 'trees' vs 'open?'). The day brings *strong sun vs shade* contrasts to the two (treeless) different sidewalks— desirable or undesirable extremes— depending on seasons. (Mid afternoon shadows stretch long across west side of corridor. East sidewalk with some awning use is in full sun.).

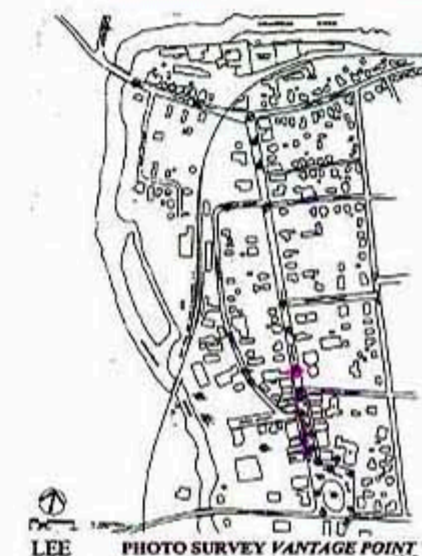
Note: Left-side structures' are larger scale, hard-faced facades, more active— vs the wooden, smaller scale quieter right side. Angle parking (lower left); parallel parking occurs remaining street).

• **Special Observational Notes:** In photo background, the southend lowers — as distant 'south hill' looms and encloses, with parting sky, the downtown: it is most enclosing when experienced from southend (due to ground/wall proportions). (Conifers create a perfect centered 'cupping terminus' to the street quad— especially prominent when there is snow). Note here the interesting prominent *descending* grade of groundplane, terminating at south node— while cornice lines of buildings, ironically, *rise* at southend (enhancing effect of st zones). ((Utility line drapes counter & harmonious *diagonal crossing angles* across Main Street to alternating swan lights (see photos for theme's variation)— as a rhythmic subspatial articulator through out the corridor (see also note re visual/ scaling/ evaluation)).

• Foreground *steeple* here is 1/3 emergent: (is a distinct powerful datum; compare changes in other photos). Also, the *hill's profile* has a horizontal kinship with the built town; and, the counter angle of hill is dynamic vs the foreground man-made verticals, (and its profile focuses toward river). (Added *trees* may bring enhancement of this as well as further spatial dynamism, along with desirable other aesthetic/climatic/ connection effects). (See previous note regarding tree strategy issues).



+ In the next several photos as we northward climb Main Street, the reader may enjoy the powerful sequential change of visual/ spatial/ human activity that gives Lee such an incredible dynamic character— (apart from eg traffic conflict).
 "As we depart from the more active, significant built-structured, most enclosed Southend (lower terrace)— the *landform base of Main Street has a steady rise, and upper plateaus* (climaxing in elevation toward the northend)— so the incredible sense of distant surrounding natural *greater Berkshire landscape becomes more present & dominant*— and we feel the truth that the town is indeed immersed/ subsumed in rich natural landscape base of geology, vegetation, seasonal/ microclimates, and river. This landform change coincides with the degree of built enclosure & busy commercial activity— to delineate main Street into a corridor of 3 different spatial-character zones. ((ie Specifically, in this sequence change, there is a simultaneous dynamism of three kinds that can be fully appreciated only on site through true motion in 3/d space: *Continuum change* of landform space (of both street and the background hills); the resultant *divisions* of three Main Street character/activity zones that successively engage foreground; these two are mediated by *incremental shifts* (and juxtapositions) of midground datums that mark dramatic relational, steeples, conifer)). "The simple fixed grid axially and euclidean structures street, and variance of natural and special elements— engage each other in space-time Bracken relativity".



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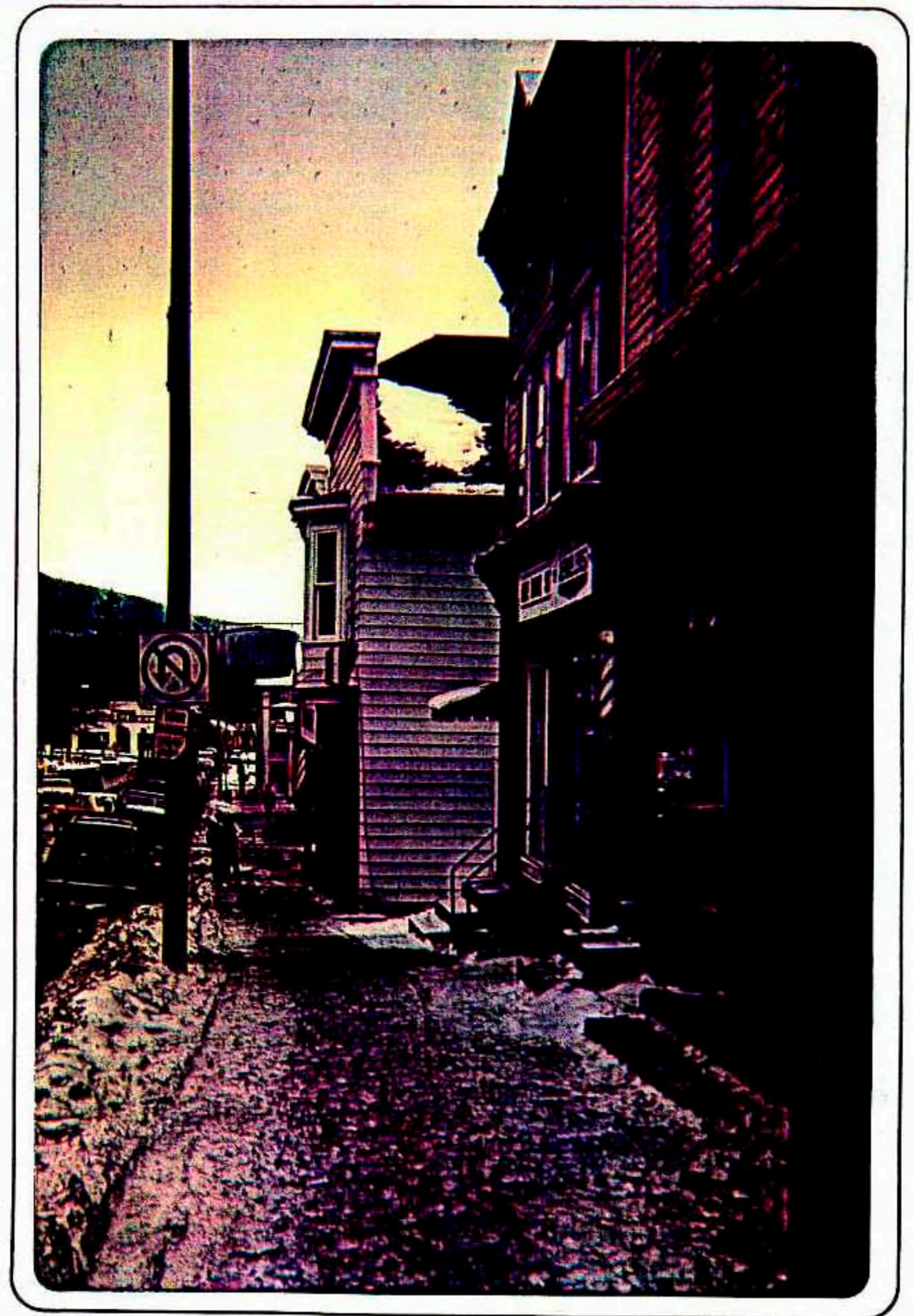


21c: (S) View opposite Library of Main Street southend west sidewalk:

Positive and negative aspects are inherent in such irregular narrow, twisting, sumping walkway space (that continues into distance).
 (+) The narrowness increases a good 'wayfaring' sense of extra length, ("like an old N/E walk, or analogous to a Berkshire trail"). This sidewalk area of street has authentic local scale intimate idiosyncrasies to appreciate— in space & material: the horizontality, the expressions of natural vertical grade, light/shade, & individuality of objects and of structures (vs each other and distinct from the more active public commercial e area of town): an important spirit of place to be preserved. But, as elsewhere (-) this area limits walking function/comfort, and prohibits pedestrian uses eg resting, vending, meeting. (There is debatable design issue re desirable amount of non-symmetry with vs the busier east walk).

(From foreground): Barber Shop, Lee Main Street Jewelers, Lee-Hardware; with upstairs office or resident options. (False roof-facade is now readable as historically honest). Theme of varied vernacular granite & wood steps, iron rail— reveal good grade character of hill, as structures claim personable defensible step-down delineation with step up to interiors. Good articulation of entries, enclaves window/ facades/ personable signage. Alleys adjacent// see 19. Eaton Street (at midphoto) has important access to improved off-street parking area— most convenient approached south bound). Parallel parking (1 hr) is convenient, but off-street uses need be overall encouraged for busy times. Service is on street and rear. Note: Awnings and recessions/ exist, but limited— less than east This sidewalk has shady afternoon/ morning sun— pleasant on hot afternoons; gloomy in winter, but some good wind protection; (Proposals can strategically use in complement to east sidewalk). Some barricade of snow storage— melts slow here. Crosswalk/ HC lacking. Walk is treeless (but see note re open/tree strategy issues). Interstate pole (no local lamps— see note regarding pos/neg layering interpretations).

* Observe the distant conifer is so large and well placed that even in from here (when vehicles subside) it reads significantly, against the backdrop repose of (south) Berkshire hill— is something to enhance along with a simultaneous lateral (westward) connection to the now hidden river area.



Survey current photos-- are by studio team member/s. (See notes).

PhotoSurvey (part a) —basic descriptions plus a flow of impressions—
continues (see part-b for next download).